

CLEARFIELD CONNECTED 2023

STATION AREA PLAN + DESIGN GUIDELINES **DRAFT**



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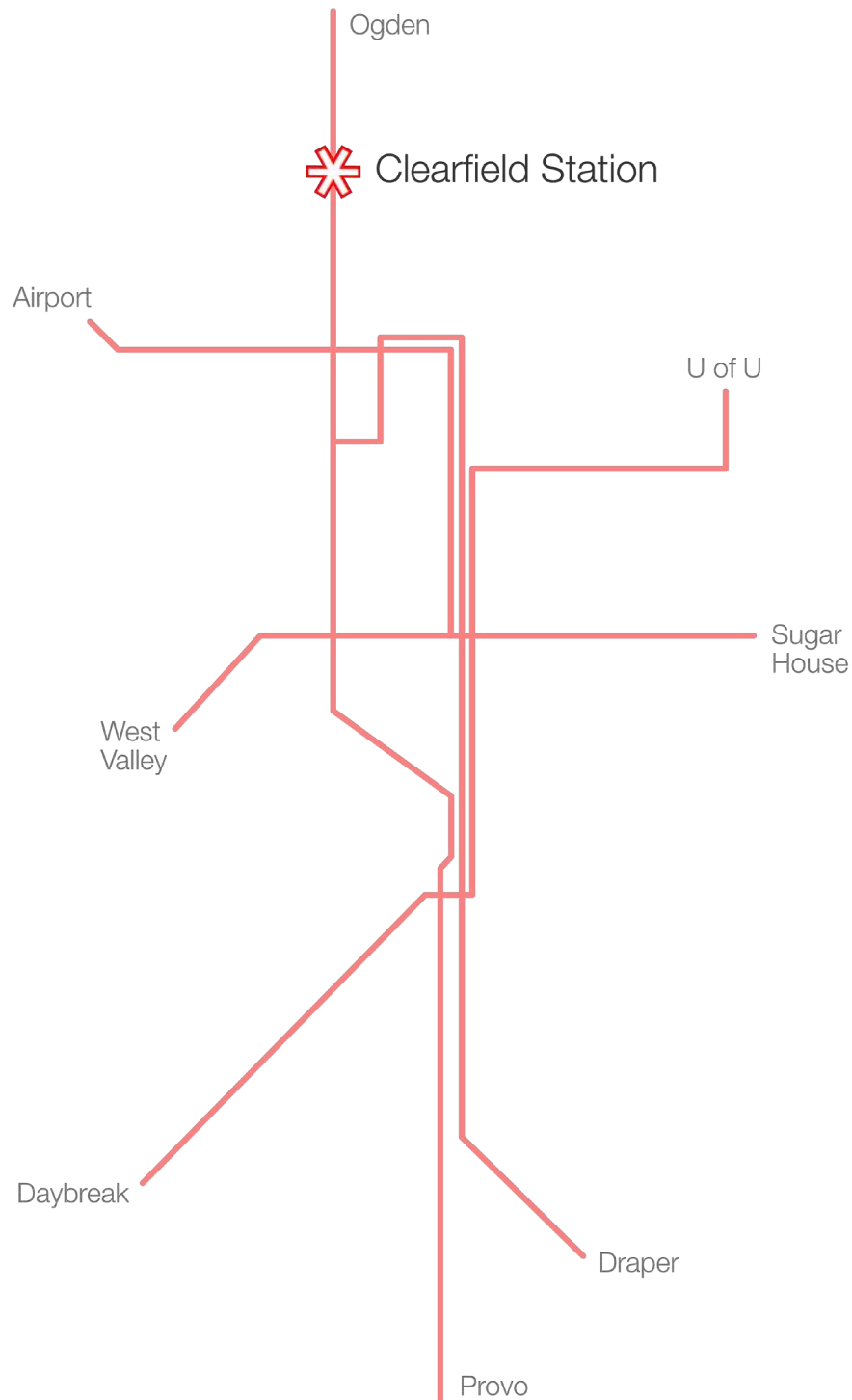
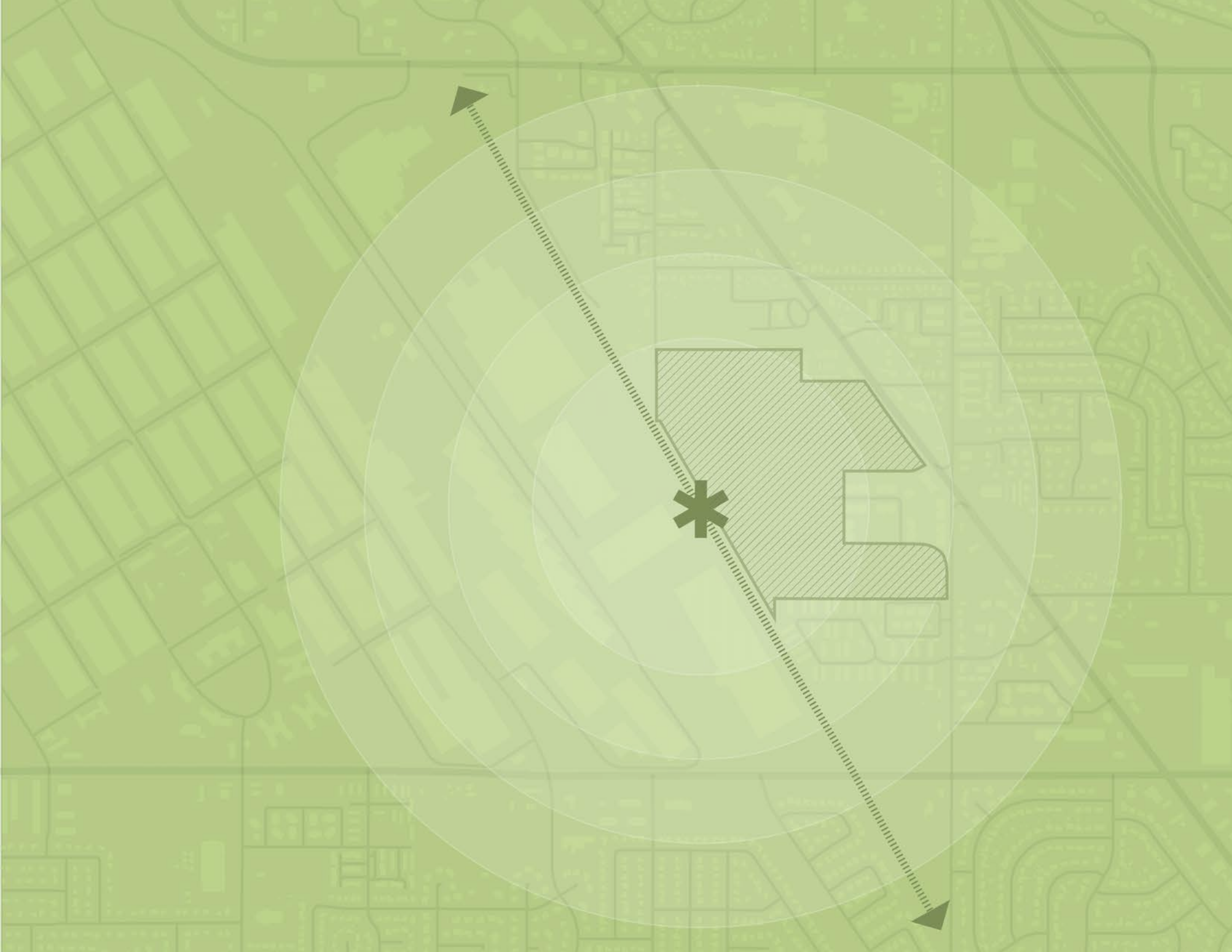


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01

INTRODUCTION

Introduction

Clearfield Connected 2023 is an update of Clearfield Connected, which was adopted in 2019. The new plan updates the vision, details and design guidelines for the Clearfield Station Area, while addressing subsequent development changes and new Station Area planning requirements recently established by the State of Utah.

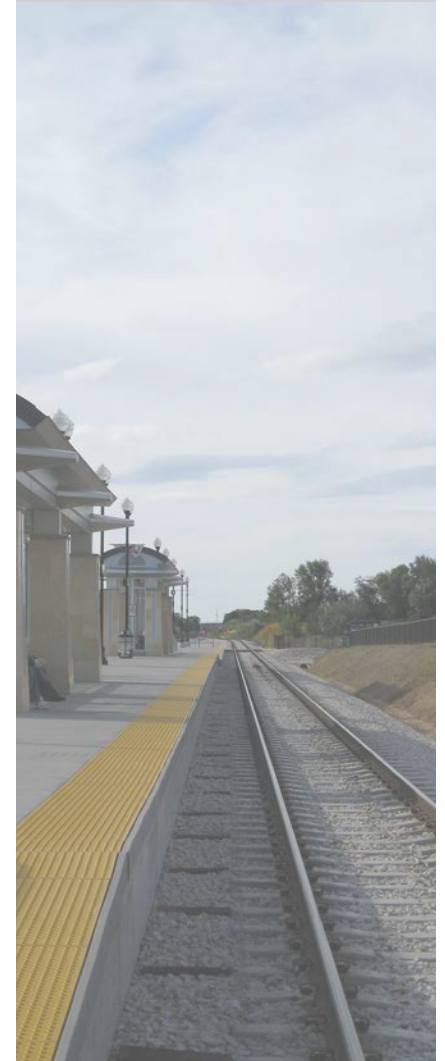
Clearfield Connected 2023 establishes the needs and vision for the FrontRunner rail system and the Clearfield Station Area, which encompasses approximately 56 acres of vacant land. In addition to meeting recent state code requirements, the Station Area plan also incorporates visioning and design elements from the Station Area Master Development Plan (MDP), which were completed in 2020 and executed between Clearfield City, UTA and the Hamilton Partners and Stack Real Estate master development team.

Clearfield Connected 2023 is a significant opportunity to meet the transit and place-making needs of Clearfield City and its residents, as well as those of UTA, the State of Utah and transit riders throughout the region. It builds upon the planning process established in the 2019 plan, expanding the vision and scope. It also establishes clear implementation principles and design guidelines to help regulate the form and quality of the area.

Clearfield Connected 2023 presents a more comprehensive vision for the area than the 2019 plan. It is fully-aligned with the comprehensive planning needs of Clearfield City, UTA and the State of Utah. Once implemented, the station and its surrounding area can leverage the benefits of current and future growth, and in the process be better connected with regional needs and changes.

DOCUMENT OVERVIEW

The purpose of **Clearfield Connected 2023** is to establish the vision, goals, urban design principles, and design guidelines that will govern future development of the Clearfield Station Area. This document lays out the structural and regulatory structure that will guide the development of the Clearfield Station Area. Graphic depictions and photos are included to help illustrate general ideas, principles, and visions for the building elements and spatial character of the station and surroundings.



COMMUNITY & STAKEHOLDER ENGAGEMENT

A comprehensive outreach strategy was utilized throughout the planning process to collect multiple levels of focused input from the public and specific individuals, groups, and stakeholders.

A Steering Committee, composed of representatives from City leadership, UTA, development partners, and other key stakeholders, met with the planning team three times at key points during the planning process.

A Plan Alternatives Public Open House was held at the Clearfield Aquatics and Fitness Center on June 28, 2023. City residents and stakeholders connected with city leaders, staff, and the planning team to learn more about the project and provide feedback on three alternative concepts. Posters were left on display for an additional week following the meeting so residents could continue to provide feedback. City staff also took the boards to Clearfield's Freedom Festival on the Fourth of July. Though the total number of participants is unknown, it is estimated that at least fifty people gave feedback during this period.

A Draft Plan Public Open House was held on November 13, 2023 at the Clearfield Aquatics and Fitness Center, providing an opportunity for residents and stakeholders to learn more about the Draft Plan and provide feedback prior to the adoption process.

A dedicated project website served as a clearinghouse for information and project updates and included comment forms for the community and stakeholders to provide feedback virtually.



Images from the Alternatives Open House on June 28, 2023

Meeting State Requirements

Recent changes in Utah State planning codes require the Clearfield Connected Station Area Plan (2019) be amended to address a wider service area and to incorporate options for affordable housing. The updated plan embraces previous efforts, translating the energy underpinning those plans into an updated and comprehensive plan that also addresses the new elements required by state code.

The updated Clearfield Connected Station Area Plan specifically encompasses the following additions and modifications:

- Assessment of prior studies and the existing conditions of the study area, focusing on the expanded Station Area “zone of influence,” changing development patterns, and recent demographic and socio-economic changes.
- Incorporation of statewide objectives for moderate-income housing, environmental conditions, transportation choices, and access to opportunities.
- Updated design guidelines that better align with the MDP.
- Assessment of the Station Area’s market potential and the synergies of commercial and multi-family residential uses, as part of a mixed-use transit district.
- Assessment of access to and from the Station Area for vehicles, transit, and active transportation modes, including pedestrians and bicyclists.



Context

HISTORIC CONTEXT

Clearfield was settled in 1877 as an agricultural community. The structure of the city began to change in the 1940's, when major defense facilities such as Hill Field and the Clearfield Naval Supply Depot were constructed within and adjacent to the city. Construction on Hill Air Force Base began in 1940, and the base soon became one of the most significant employers in the region. The air base remains one of the largest employers in the state, and continues to employ many local residents.

The Clearfield Naval Supply Depot was constructed in 1942 adjacent to the railways that line the west edge of Clearfield Station today. The depot also became a major employer, but was decommissioned in 1962. The remnant facilities of the depot eventually became the Freeport Center, which is now a major manufacturing, warehousing, and distribution center.

The city is a major employment center and home to many large companies, many of which are located in or around the Freeport Center.

The Clearfield Station site is east of the railroad tracks and has historically been used for light industrial uses.



Naval Supply Depot, 1942 (Source: Weber State University)



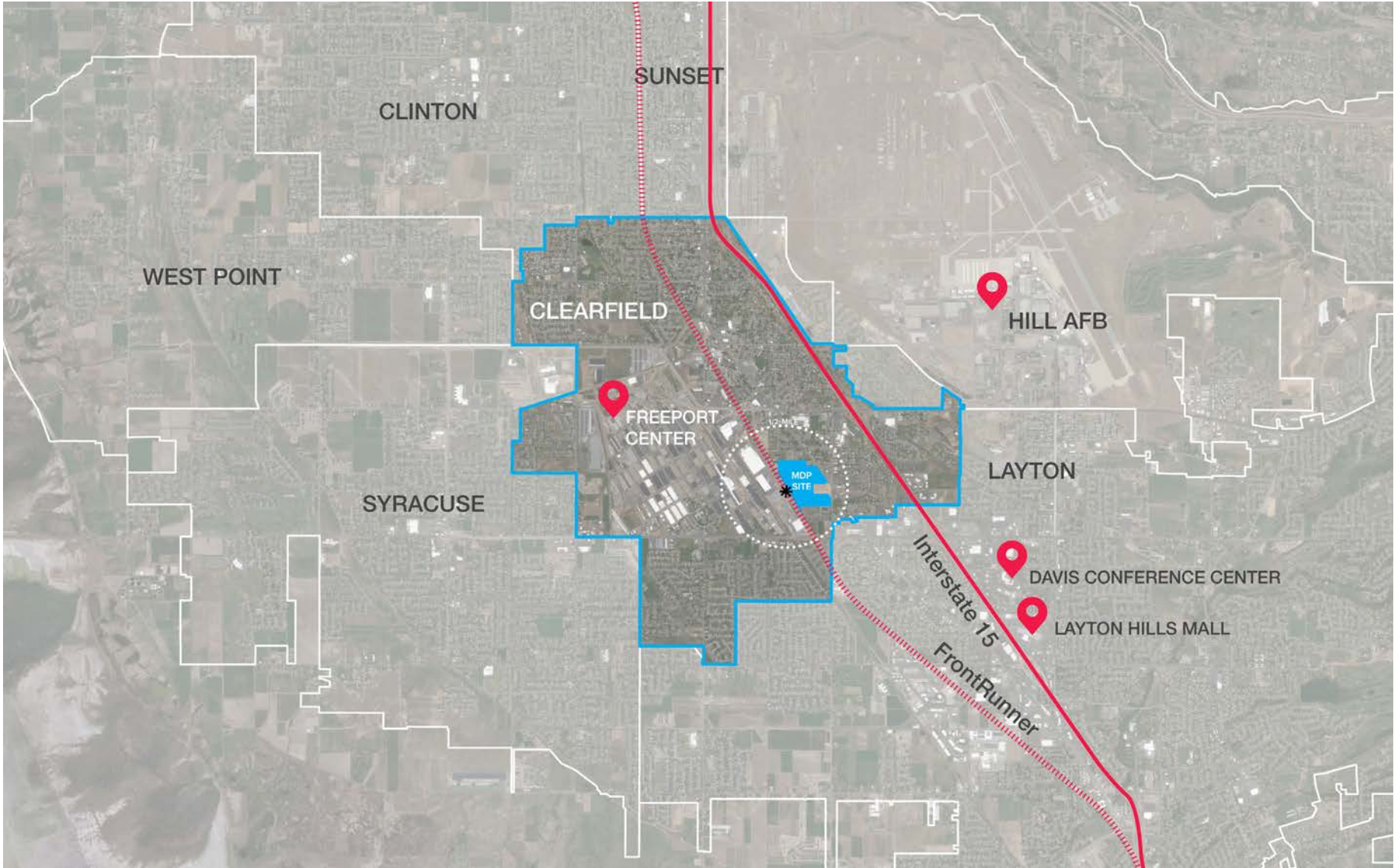
Hill Airforce Base, 1958 (Source: The Salt Lake Tribune)

REGIONAL CONTEXT

The City of Clearfield is located 28 miles north of Salt Lake City in northern Davis County. It is situated between the Great Salt Lake to the west and the Wasatch Mountains to the east, encompassing an area of about 7.7 square miles. The city is located in a key location southwest of Hill Air Force Base—the State's largest economic engine.

Interstate-15 runs along the eastern reaches of the city, providing interchanges at 650 North and 700 South / SR 193. 700 South and Antelope Drive are the largest corridors for east-west traffic movement in northern Davis County. Clearfield lies 30 miles north of the Salt Lake International Airport.

REGIONAL CONTEXT MAP



The Clearfield Station Area

The Clearfield FrontRunner Station is one of sixteen stops along the Frontrunner commuter rail line that runs approximately 90 miles along the Wasatch Front, connecting users between Ogden in the north and Provo to the south. The rail line has established Clearfield Station as a key regional connection.

The Clearfield Station Area (also known as the Station Zone of Influence) includes all parcels within a half mile radius of the Clearfield Station. As illustrated in the Local Context Map on the following page, it encompasses the UTA-owned MDP site and extends into the surrounding neighborhoods. It also includes a portion of the Freeport Center and commercial properties along State Street and Antelope Drive.

The MDP site encompasses approximately 56 acres of undeveloped land between the rail line/FrontRunner tracks and State Street. It contains the largest amount of vacant UTA-owned land adjacent to a FrontRunner or TRAX transit station in the entire UTA system. The site is currently used as a park-and-ride lot for transit riders, with new roads and a few structures currently under construction. As mentioned previously, this site has already been planned in the

Clearfield Station Master Development Plan (MDP).

This plan incorporates the existing neighborhoods within the Zone of Influence into the overall design of the Station Area, while capitalizing on opportunities for positive transformation. At buildout, the Clearfield Station Area will be a cohesive neighborhood that seamlessly incorporates existing apartments and other established uses into the overall structure of the area.

VEHICULAR ACCESS

Access to Interstate-15 is available approximately one-mile northeast of the MDP site along 700 South, and to the southeast along Antelope Drive. State Street (SR 126) is a major north/south arterial that fronts the site to the east and provides access to Clearfield City Center in the north and the greater Wasatch Front region north and south. The Salt Lake International Airport is located approximately 30 miles south of the site and is easily accessible via I-15/Legacy Highway and by FrontRunner with a direct connection along the TRAX light rail system. Local traffic in proximity to the Station Area is controlled by a signal located at the

intersection of 1000 East and State Street and will be controlled with proposed intersections at Station Boulevard and 1450 South later on as the MDP site develops.

PEDESTRIAN & BICYCLE ACCESS

The Denver and Rio Grande Western Rail Trail is a dedicated active transportation facility within the Station Area. This paved facility is part of the Golden Spoke Route and US Bike Route 77, providing trail connections north to Ogden and south to Provo. There are several planned active transportation line and point projects in the area, according to the North Davis Active Transportation Plan and the 2023 WFRC RTP.

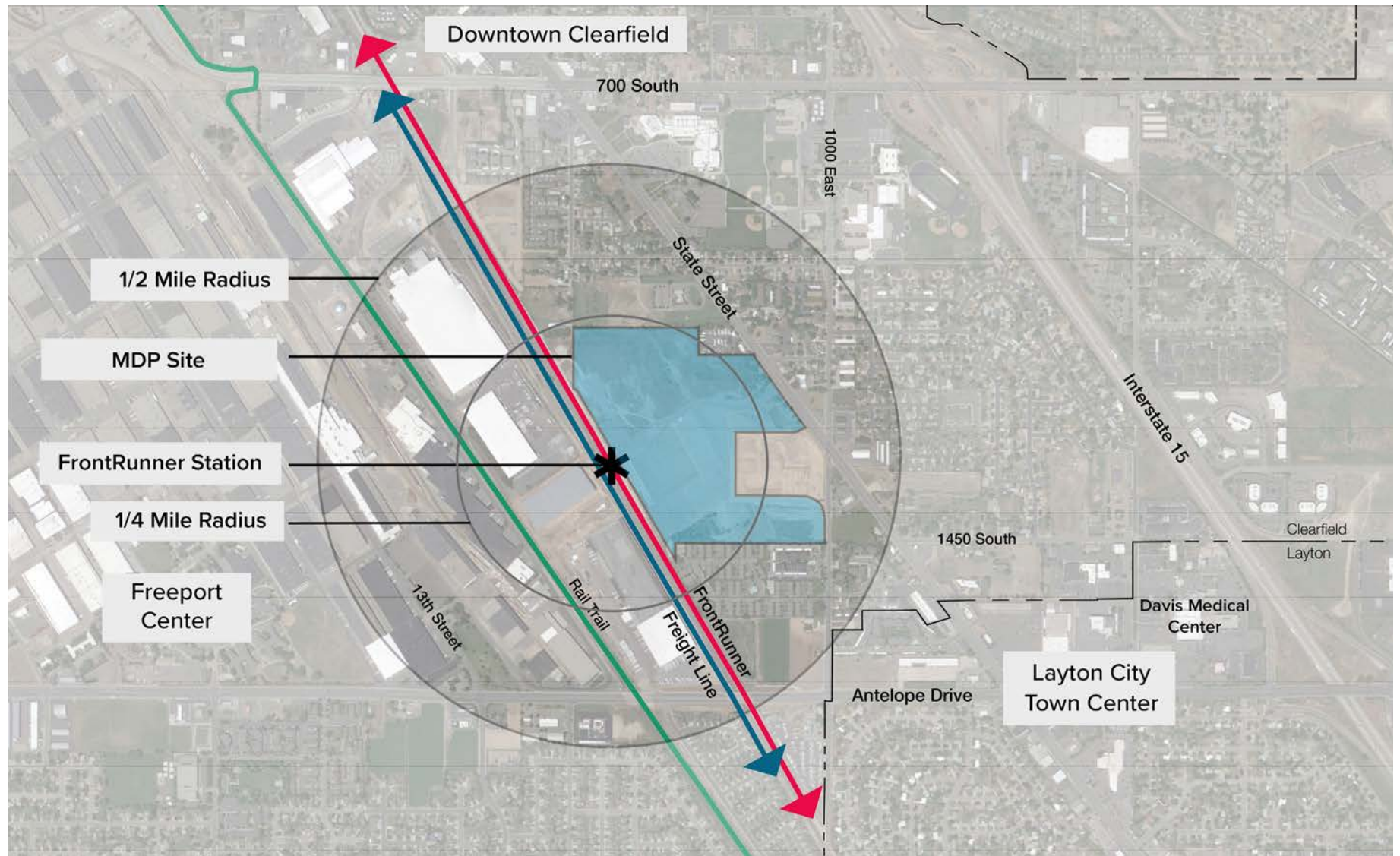
Bike lanes are planned for Depot Street, 1000 East, 1450 South, 700 South, on Antelope Drive west of 1000 East, and Station Boulevard. Additional planned projects include a protected bike lane on State Street, a trail connection from the FrontRunner Station south to Antelope Drive, a shared-use path on Antelope Drive west of 1000 East, and neighborhood byways on 1150/1100 South.

Other planned pedestrian and bicycle enhancements include at-grade pedestrian/bike crossings at 1150 South State Street and at 1000 East and Antelope Drive, and a planned at-grade trail connection between the Denver and Rio Grande Western Rail Trail and the planned shared-use path on Antelope Drive.

The site is connected to the rest of the City through streets and sidewalks on the east side of the property, although the connections are currently limited. The multi-family development on the south of the site is currently separated by a fence with no connections provided into the site. The north boundary of the site currently lacks any connections, although Depot Street is proposed to connect to the site, allowing vehicular, pedestrian, and bicycle connections to the north.

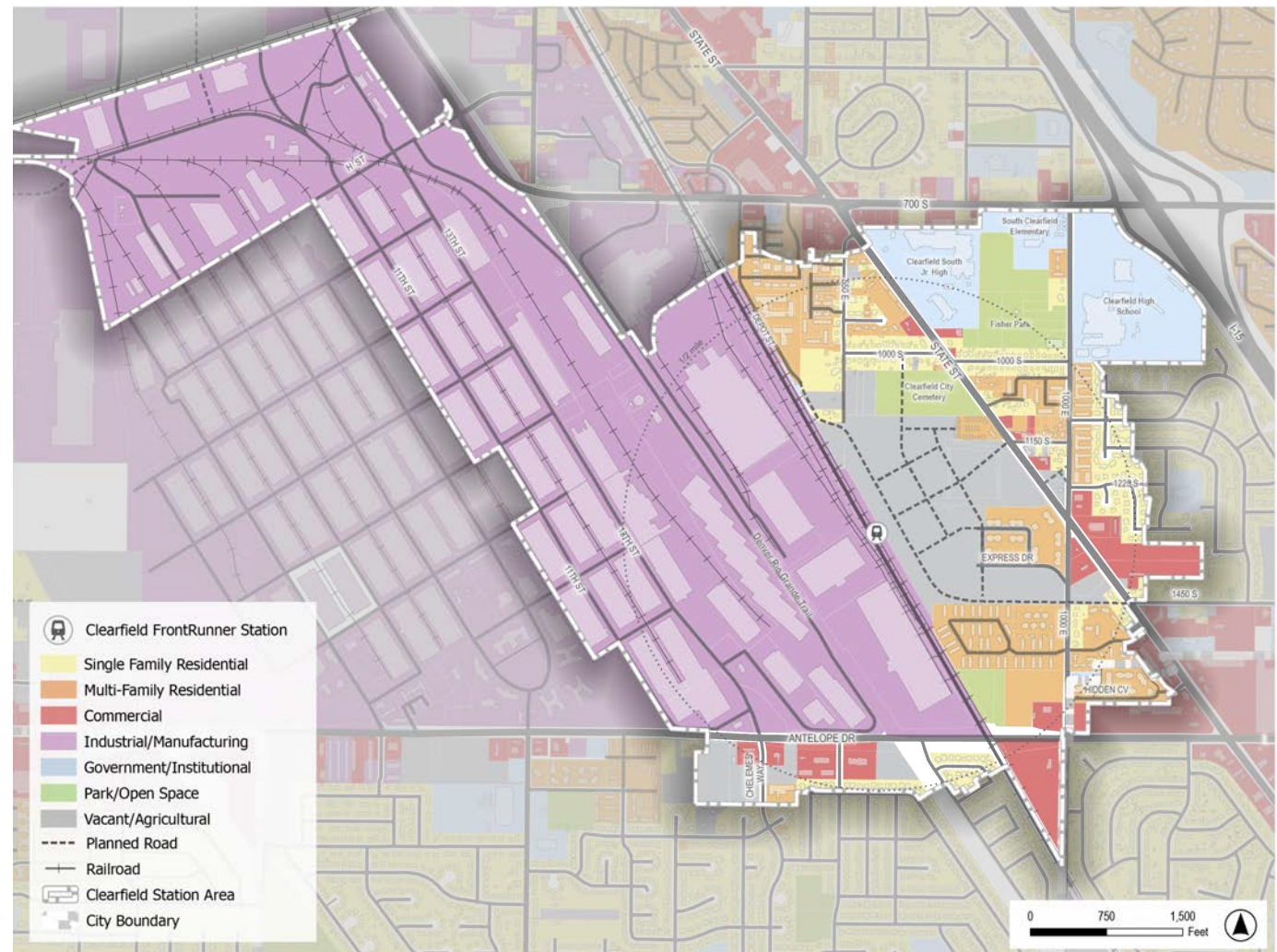
There is very limited access to the property from the Freeport Center to the west of the property. The rail lines adjacent to the site are significant barriers, preventing direct pedestrian and cycle linkages to the Station Area. Similar access and crossing challenges exist along State Street, Antelope Drive and 700 South, due to the heavy traffic and lack of bike / pedestrian infrastructure.

LOCAL CONTEXT MAP



Existing Land Use + Ownership

The accompanying map shows the general land-uses that encompass the Station Area. To summarize, the MDP site is currently owned by the Utah Transit Authority (UTA). Existing parking lots are legally non-conforming uses with maintenance rights. Current land uses surrounding the site are primarily single-family and medium-density residential housing. East of the site is the State Street commercial corridor. The Freeport Center is to the west, which hosts a variety of industrial uses including processing, assembling, manufacturing and warehouse storage. A handful of commercial uses are located on the south side of Antelope Drive.



Existing Conditions Analysis

LAND USE

With a limited amount of vacant land remaining in the Station Zone of Influence, most development is expected to occur within the MDP site. However, opportunity exists for transitional land uses along the edges of the site, which would support implementation of the MDP and help create a more complete station district. The map to the right highlights these sites as Potential Transformation Areas.

TRANSPORTATION

The Clearfield Station Area is currently auto oriented, with little to no access with adjacent land uses. Despite this, a large percentage of station users are pedestrians, even though there has been little infrastructure to support it. Recent infrastructure improvements to the MDP site will help better support pedestrians and cyclists.

Planned trails to the north and south of the station will help accommodate active transportation users, particularly the direct connection to the Denver and Rio Grande Rail Trail.

Overcoming active transportation barriers across State Street through

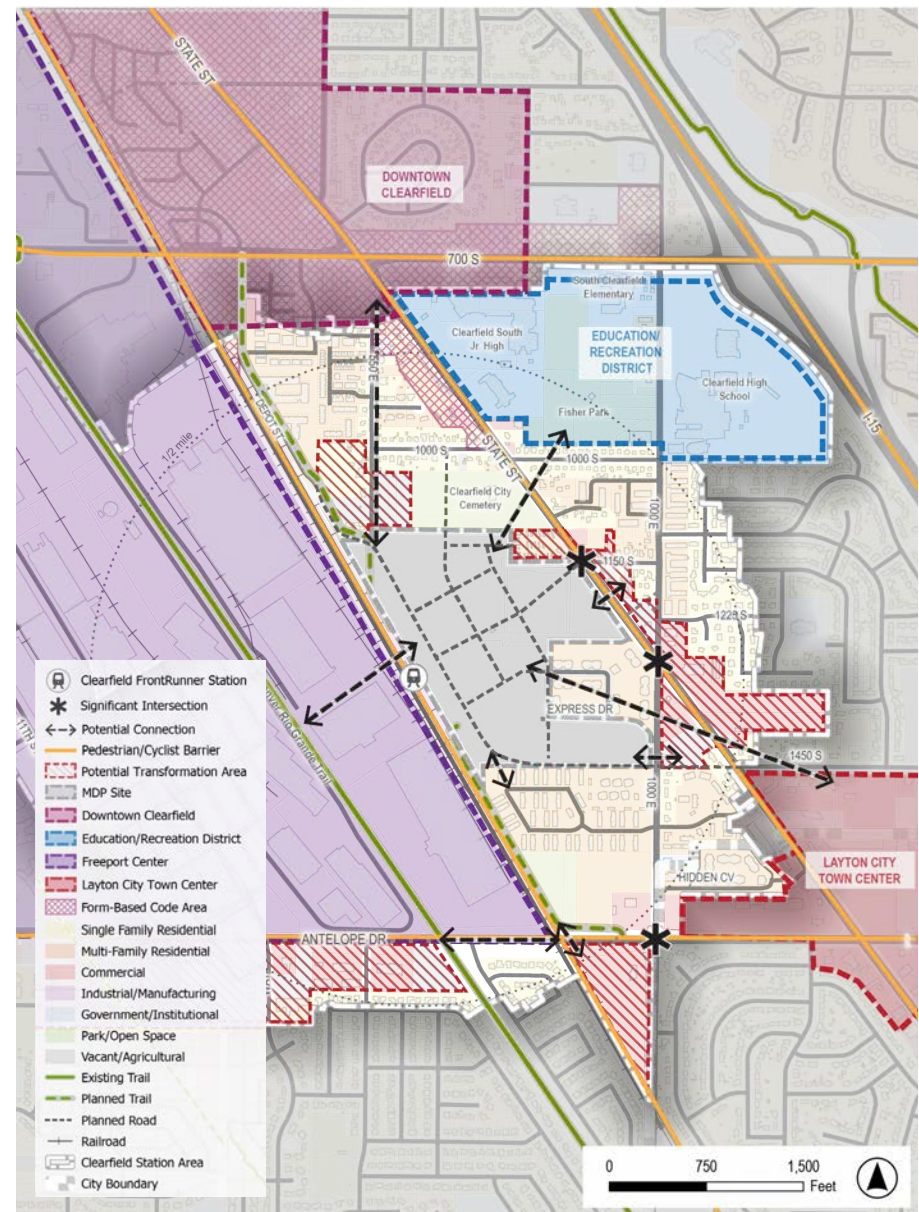
well-planned crossings will be key to providing meaningful connections to areas east of the station.

MARKET CONDITIONS

Clearfield City is a regional employment center with employment expected to continue to grow over the coming decades. Northern Davis County is projected to add 20,000 more jobs by 2040.

The city is only capturing 41% of its expected taxable sales for its population. The office space market is experiencing a slow down, with vacancy rates on the rise and negative absorption rates in 2022. The greatest market demand is for residential, flex office, and flex industrial. Strong population and employment growth are also fueling demand for retail. Retail will be the highest revenue generator for the city.

For a more detailed assessment on existing conditions see Appendix A: Existing Conditions Report.



The Need for an Updated Plan. The Potential for this Area.

Why Here? Why Now?

The current development market is thriving and this area possesses a unique mix of factors that could come together to make it a highly sought after development opportunity. The following features and factors clearly illustrate the extraordinary opportunities offered in the Clearfield Station Area, and the favorable external factors that make conditions prime for quality development.

THE FRONTRUNNER STATION

The FrontRunner Station is an incredible asset for Clearfield, as it connects the City to much of the Wasatch Front. Together with the bus system and other transit choices, it provides residents with the option of commuting and getting around the region without a car.



POPULATION GROWTH

As one of the fastest growing states in the country, Utah is expected to grow another 50% by 2040. Unfortunately, rapid growth has led to a lack of housing, which has resulted in significantly increased housing costs in recent years. This has led to a strong demand for more housing, most particularly compact and efficient multi-family residences. There is also a specific need for multi-family housing, which is most effective in high-quality, mixed-use neighborhoods.



STRONG ECONOMIC CONDITIONS

Utah currently has one of the strongest economies in the nation and is one of the fastest growing states in the nation. There is strong pressure for growth in both housing and employment opportunities.



ECONOMIC INCENTIVES

The Station Area is eligible for significant economic incentives that will help make the high-quality development that this document envisions financially feasible. Some of the key programs include funding incentives such as the local RDA/CRA that is currently in place, as well as the federally designated Opportunity Zone incentives that this area is eligible for.



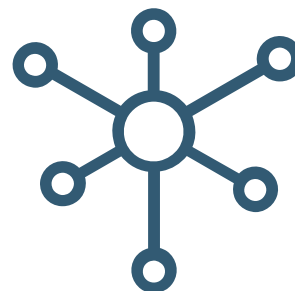
COMMUNITY ASSETS

The development of offices and housing in this area will generate demand for amenities that will provide benefits not only for residents and employees of the Station Area, but for the City as a whole. Anticipated amenities include high-quality public open space, enhanced street amenities, retail shops and restaurants, and similar uses and features.



REGIONAL HUB

The station is located across the railroad tracks from the Freeport Center near the Clearfield-Layton border. It is also close to Hill Air Force Base (northeast), Holy Cross Hospital - Davis (southeast), Downtown Clearfield (north), the planned Layton City Town Center (south), and an education/recreation district composed of three public schools and a park to the northeast.



OPPORTUNITY TO CREATE SOMETHING GREAT

The Station Area provides an opportunity to create something great in Clearfield and Northern Davis County. A thoughtful, collaborative Station Area plan that is based on market realities will encourage interest from the development community to create a great place that will help put Clearfield on the map.



Transit Oriented Development (TOD)

WHAT IS TOD?

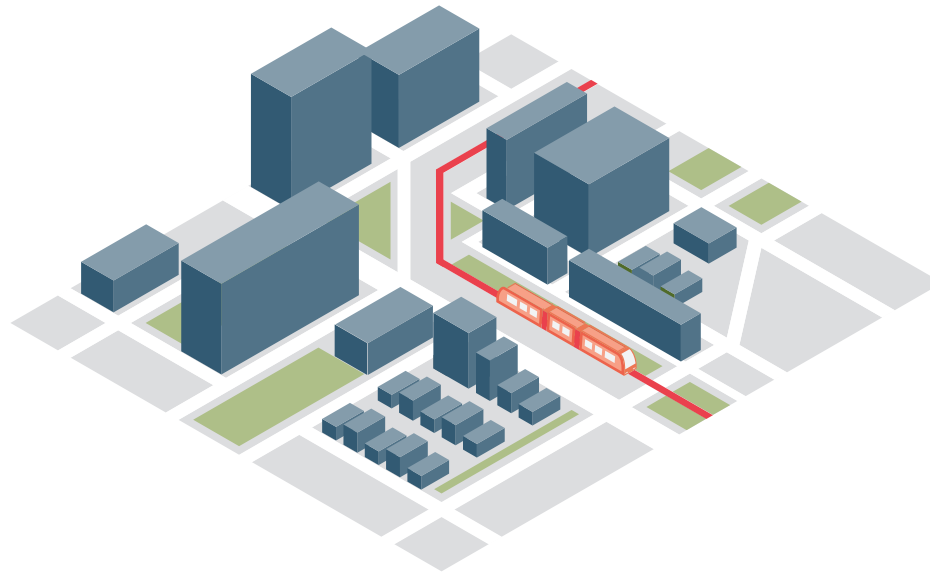
With its direct connection to a major transit station, the Clearfield Station Area is ideally suited for Transit-Oriented Development, which is essentially a development strategy that aims to make the most of the development possibilities near a major transit station. It is defined by Reconnecting America, one of the leading TOD organizations, as “a type of community development that includes a mixture of housing, office, retail and/ or other amenities integrated into a walkable neighborhood located within a half-mile of high quality public transit.”

WHAT’S DIFFERENT ABOUT TOD?

For decades, cities have often segregated uses, with single family homes, multifamily homes, offices, retail, civic uses, and more all zoned into their own areas within the larger city. TOD takes a different approach by mixing compatible uses in each neighborhood or city district, which is more akin to the way cities formed before cars became prevalent, and allowing residents to travel long distances between home, work, and other destinations. TOD leverages access to public transportation to create districts where transit, walking, biking, and other modes of transportation come together to create neighborhoods that hearken back to traditional cities and villages. The results are not only great places to live and work, but great destinations that are walkable, unique and provide a close-knit community feel.



Elements of Transit Oriented Development (TOD)



ELEMENTS OF TOD

The major elements of a TOD can be broken down into three categories (which conveniently correspond with the TOD acronym).

- Transportation
- Open Space
- Development

TRANSPORTATION **T**

TOD brings a range of transportation modes together. Transit, walking, bicycling, driving, and similar modes are served by specially-designed infrastructure and amenities (lanes, parking, transit stops, stations, sidewalks, etc.) that allow residents and visitors to travel safely, conveniently, and comfortably, regardless of the selected mode they choose.

OPEN SPACE **O**

Public spaces (i.e. plazas, patios, parks, and sidewalks) form the places between transportation facilities and buildings of the Station Area. These are where the life of the station and city play out and where people come together. Open space can be public or private, but should always be designed to be accessible, user-friendly, attractive, and fun for all.

DEVELOPMENT **D**

These are the buildings and structures where a range of human activities take place. A well-designed mix of housing, employment, shopping, and other uses are the core of station development. This mix results in appropriately-scaled and well-designed buildings that relate to and activate the surrounding open spaces and streets and support transit ridership with essential density.

UTA Goals for TOD

UTA GOALS

UTA-owned land near transit stations must be developed in accordance with Transit-Oriented Development Design Guidelines adopted by the agency. These provide direction for joint-development partners on the design elements that UTA expects to be addressed in development plans, such as connectivity and development form.

Unlike other typical land owners, UTA has development expectations and goals that extend beyond making a profit. As a public transit provider with a clear objective to generate the best return from their investments possible, UTA is also charged with maintaining a strong relation between its property development and public service activities. All development on UTA-owned land near UTA stations is carefully reviewed by UTA staff to ensure compatibility with these goals. Local jurisdictional codes must also be followed when developing plans to ensure they are not in conflict with UTA guidelines.

Clearfield Connected 2023 and the design guidelines it contains have been created to be in accordance with the following goals and UTA's Transit-Oriented Development Design Guidelines. While meeting these goals

can be challenging, staying the course will ensure that UTA continues to fulfill its responsibility to the public as a world-class transit operator, which in turn will make TOD not only feasible but a preferred model for future development.

GOAL 1: INCREASE RIDERSHIP

UTA understands that the real estate market drives development feasibility. In fact, appropriately designed residential and employment centers can generate significant increases in ridership. As a result, both vertical and horizontal mixed uses are strongly encouraged at in Station Areas.

Unfortunately, some land uses simply do not generate the level of ridership UTA expects for TOD. For example, an employment center that has low worker densities or hours of operation do not allow workers to utilize the transit system for commuting and are not considered transit supportive. The primary objective of UTA is to maximize the public transit investment at their Station Areas.

GOAL 2: OPTIMIZE DEVELOPABLE LAND AND SUPPORT THE REGIONAL GROWTH VISION

Helping to meet the challenges of rapid population growth along the Wasatch Front is a critical goal for UTA. Land uses that reduce the negative impact of this growth are at the heart of the UTA TOD program. This includes support for the 3% Strategy developed by Envision Utah, which calls for 33% of future development to occur on 3% of available land. It also supports the Wasatch Choice Vision, that calls for the development of higher density “centers” and “corridors” across the Wasatch Front that are served by high capacity transit.

Both strategies were developed with tremendous public input and regional coordination, and address issues like poor air quality, traffic congestion, auto dependency, and housing equity. They also support regional economic development and improved access to transit through first and last mile strategies.

GOAL 3: GENERATE REVENUE

Like any property owner and development partner, UTA expects to realize a suitable return when developing its property. While UTA receives most of its operating revenue from a local option sales tax, joint-development is seen as a new and innovative revenue approach to help fund future improvements and operations.



Design Guidelines Overview

INTENT

This document contains design guidelines that regulate development in Clearfield Station Area. The design guidelines correspond with the TOD elements outlined on page 18, and are found in the Transportation + Mobility (T), Open Space + Public Realm (O), and Buildings + Architecture (D), sections of this document.

The intent of the Design Guidelines is to establish strong urban design principles and quality development, while also establishing a clear and coherent design theme and a consistent look and feel throughout the Clearfield Station Area.

The guidelines provide a design vocabulary that is unique to Clearfield Station. They promote a sense of aesthetic continuity, ensure high quality development, and help establish a clear and distinct community identity.

DESIGN REVIEW COMMITTEE (DRC)

A Design Review Committee (DRC) should be established to review all development in the Clearfield Station Area to verify each project meets the vision for the greater Station Area. It is also the responsibility of the DRC to ensure all applicable design guidelines are followed.

INTENT STATEMENT

The intent statement establishes the over-arching design intent for each category or topic. This has been structured to help designers understand the rationale and aspirations that lie behind the design guidelines. In the event the guidelines and standards are not clear or appropriate, the intent statement shall be referenced as the primary source of direction for project designers and the Design Review Committee (DRC).

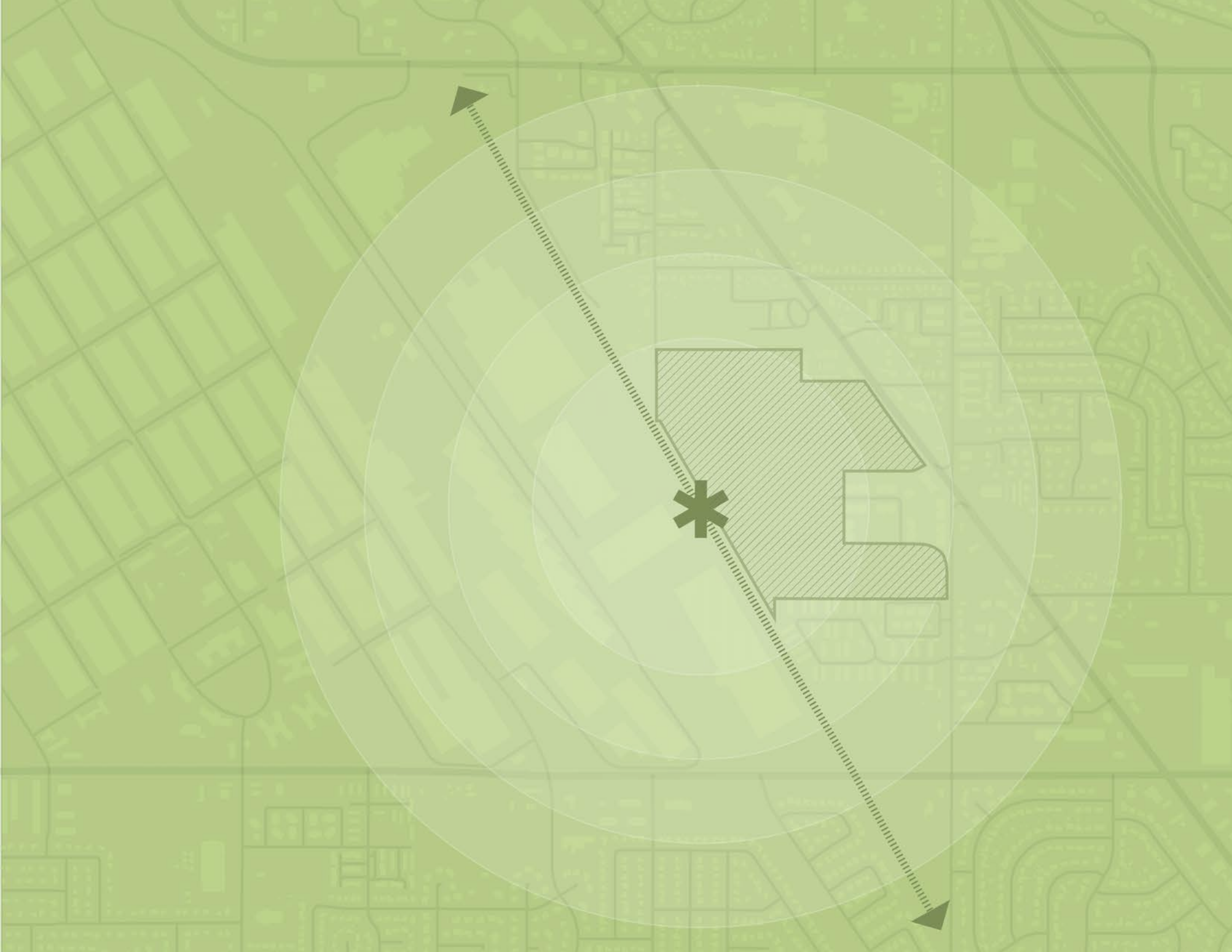
DESIGN GUIDELINES

The design guidelines provide specific direction that designers should reflect in their projects. The guidelines ensure that a level of consistency is achieved across the various projects that will occur at the station and surrounding areas, thereby helping all participants in the design and development process achieve a sustained and even level of quality.

The design guidelines typically use the term “should” or “may” to indicate ideas and directions that should be implemented when possible or practical. Conversely, when the word “shall” or “must” is applied, the designers and developers are required to meet the stated requirements to obtain approval from the DRC.

In the event that a guideline is not applicable or appropriate, a process is established to provide flexibility, whereby the DRC may grant exceptions if the applicant can clearly demonstrate that a more appropriate solution is consistent with the intent, vision and project goals as presented in this document.

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02

***PROJECT
VISION +
GOALS***



CLEARFIELD STATION

The Clearfield Station Area is a thriving, mixed-use, walkable neighborhood that leverages multiple transportation options to create a complete community connected to the Wasatch Front. It will become a regional destination that provides abundant opportunities for employment, living, shopping, recreation, and more, which will all merge together to create a great place.

The **12** Goals for this Project Are...

01 INCREASE THE AVAILABILITY & AFFORDABILITY OF HOUSING

02 PROMOTE SUSTAINABLE CONDITIONS & PRACTICES

03 ENHANCE ACCESS OPPORTUNITIES

04 INCREASE TRANSPORTATION CHOICES & CONNECTIONS

05 CREATE AN EXCITING DESTINATION

06 CREATE A COMPLETE COMMUNITY

07 PROVIDE COMMUNITY ASSETS

08 PROMOTE QUALITY URBAN DESIGN

09 MAINTAIN CONVENIENT TRANSIT ACCESS

10 GENERATE TRANSIT RIDERSHIP

11 CONNECT THE STATION AREA TO THE CITY + REGION

12 PROMOTE THE CITY'S INDUSTRIAL HERITAGE

Project Goals for Clearfield Station

INCREASE THE AVAILABILITY AND AFFORDABILITY OF HOUSING

As a primary TOD area in the region, Clearfield Station Area is critical for merging the affordable housing goals described in the Clearfield General Plan. It is therefore essential that the Station Area includes residential densities necessary to facilitate affordable housing options within ½ mile of the station, and in the process provide affordable living opportunities that are aligned with citywide housing and transportation goals.



PROMOTE SUSTAINABLE CONDITIONS AND PRACTICES

The Station Area and the areas that lead to it should exemplify sustainable design and development practices necessary for maintaining the environmental integrity of the city and region. Chief among these practices is the conservation of water resources through efficient land use and application of state-of-the-art practices, the improvement of air quality by reducing fuel consumption and motor vehicle trips, and establishing parks, open space, and recreational opportunities within the plan area.



ENHANCE ACCESS TO OPPORTUNITIES

The Station Area should leverage a mixed-use, TOD design approach to maintain and improve the physical and logical connections between housing, employment, education, recreation, and commerce. Enabling opportunities in proximity to the transit station should be supported through ancillary actions that provide enhanced broadband connectivity throughout the area.



INCREASE TRANSPORTATION CHOICES AND CONNECTIONS

As a regional mixed-use TOD destination, the Station Area should include the necessary infrastructure to support all modes of transportation. This will not only make better public transit investments, but also help ensure the station is a safe environment for pedestrians, cyclists, and other non-motorized modes of transportation. Such actions should be further supported through the creation of manageable and reliable traffic conditions and be aligned with regional transportation plans.



CREATE AN EXCITING DESTINATION

Clearfield Station Area provides an unique amenities that help create an exciting user experience. It will be a significant employment center and destination for people from surrounding communities and the larger Wasatch Front.

The public realm (streets and open spaces) is designed in a way that makes the neighborhood walkable and friendly, providing unique and exciting experiences for users.



CREATE A COMPLETE COMMUNITY

The Clearfield Station Area provides a mix of land-uses that work together to create a complete community. The primary land uses are office, commercial, and residential supported by retail, restaurants, food markets, public gathering spaces and other neighborhood services, all within walking distance of each other and the station.



PROVIDE COMMUNITY ASSETS

Clearfield Station Area is an asset to the larger community, providing a number of community assets such as parks, plazas, recreation facilities, and vibrant, walkable streetscapes. All development in the neighborhood should promote livability for residents and visitors.



PROMOTE QUALITY URBAN DESIGN

Clearfield Station Area is designed and planned according to sound urban design principles that promote walkable, safe, and livable streets. All development exhibits quality architecture, landscape architecture, and urban design, which is unified to create a great “place.”



MAINTAIN CONVENIENT TRANSIT ACCESS

The Clearfield FrontRunner Station continues to be a convenient and functional park-and-ride destination for nearby residents. Parking is provided in close proximity to the station platform to accommodate commuters, and the existing bus access loading/unloading zone will remain to encourage further transit ridership. Convenient automobile and bus access will be provided without jeopardizing safe pedestrian circulation. Improvements to the Station Area will enhance the user experience for park-and-ride users by providing a transit plaza with convenient retail options.



GENERATE TRANSIT RIDERSHIP

The land uses and location of new development are arranged to maximize transit ridership by locating the densest uses closest to the platform, with the least dense uses on the periphery. This also includes developing uses that act as origins and destinations for transit riders.



CONNECT THE STATION AREA TO THE CITY + REGION

Clearfield Station Area incorporates multiple transit modes that provide residents, commuters, and visitors with a variety of transportation choices that connect the Station Area to the city and region. These include commuter rail, bus, and personal vehicles, as well as safe and friendly pedestrian and cycling facilities. Additional streets are created that connect Clearfield Station to the rest of the city.

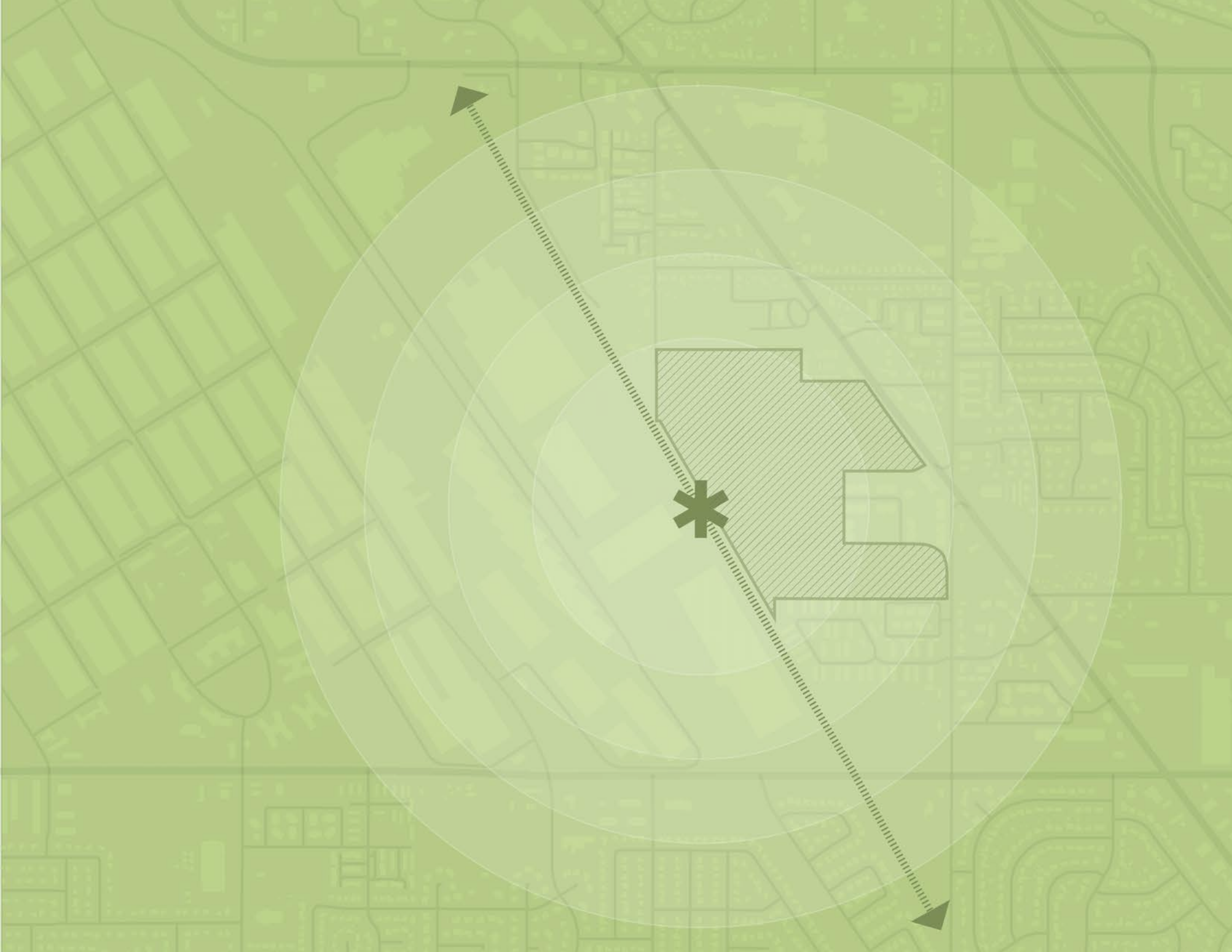


PROMOTE THE CITY'S INDUSTRIAL HERITAGE

Clearfield Station Area promotes the city's long history as an industrial job center by integrating a contemporary industrial look and feel to the architecture and design of the neighborhood. This industrial character is displayed through the spirit of the place, providing the amenities and experiences needed to support a modern-day workforce and help it perform as one of the leading employment centers in the region and state.



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03

***FRAMEWORK
+CONCEPT
PLAN***

Framework + Concept Plan

OVERVIEW

This framework and concept plan builds upon the established vision and goals (*Chapter 2: Project Vision + Goals*) and the *Existing Conditions Analysis* (see *Appendix A*). It provides a foundation for future development within the Station Area, with a focus on currently vacant and underutilized land. This concept plan includes four layers of varying detail: Districts, Framework, Future Land Use, and Illustrative Master Plan. Together these layers provide a basis for the development of a thriving walkable station district.

INTENT

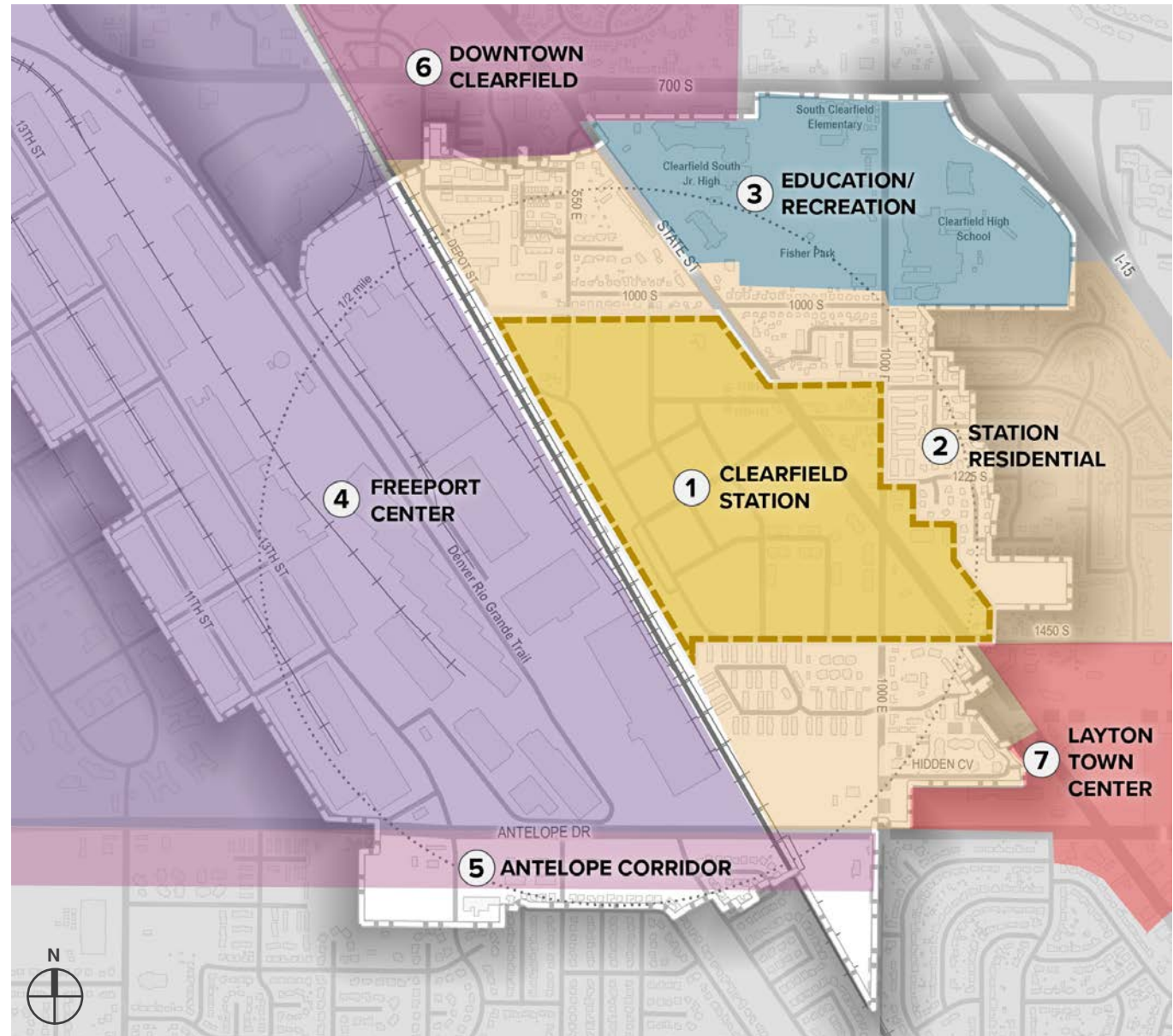
Provide a clear plan for future development of Clearfield Station Area that reflects existing conditions and the vision and goals established for the area.

ILLUSTRATIVE RENDERING: PERSPECTIVE VIEW: VILLAGE SQUARE



Districts

The station Zone of Influence is divided into five districts within the 1/2 mile Zone of Influence (Clearfield Station, Station Residential, Education/Recreation, Freeport Center, and Antelope Corridor), each with a unique character based on their land uses. The zone of influence also has relationships with two additional districts, namely Downtown Clearfield and Layton Town Center. Each district contains a unique personality established by the specific setting, character and uses.



DISTRICT CHARACTER

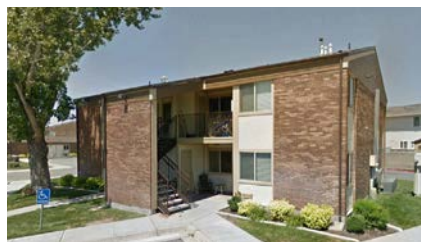
① CLEARFIELD STATION

Clearfield Station is the heart of the neighborhood and the focus of this plan. It is the most dense and active district, where people come to work, live and connect. As a mixed-use district, it provides connections between the station and office, residential, retail, and public open spaces.



② STATION RESIDENTIAL

This district encircles the Clearfield Station District on the north, east, and south. The area provides a range of residential and ancillary uses that help to create a transition between the densely developed MDP Site and lower-density areas beyond.



③ EDUCATION/RECREATION

This district offers access to a full range of K-12 public schools, including Clearfield High School, North Davis Junior High, and South Clearfield Elementary. The district also includes high-level park and recreation opportunities at the Clearfield Aquatic and Fitness Center and Fisher Park.



④ FREEPORT CENTER

This is an important and well-established industry and job generating district. Separated from the station by a north-south running regional rail line, the district is physically close but difficult to connect due to the barriers created by the rails. As a result, Freeport Center has limited effect and influence on the Clearfield Station Area.



⑤ ANTELOPE CORRIDOR

This district straddles the south edge of Antelope Drive, bringing a mix of roadway-oriented commercial and medium-density residential uses to the area. The district helps buffer the lower density residential uses directly to the south, while offering additional commercial and housing options within the greater Station Area.



⑥ DOWNTOWN CLEARFIELD

Downtown Clearfield lies just outside the Clearfield Station Area. Together, the two centers help to establish Clearfield as one of the most diverse, dynamic, and mixed-use communities in the region.



⑦ LAYTON TOWN CENTER

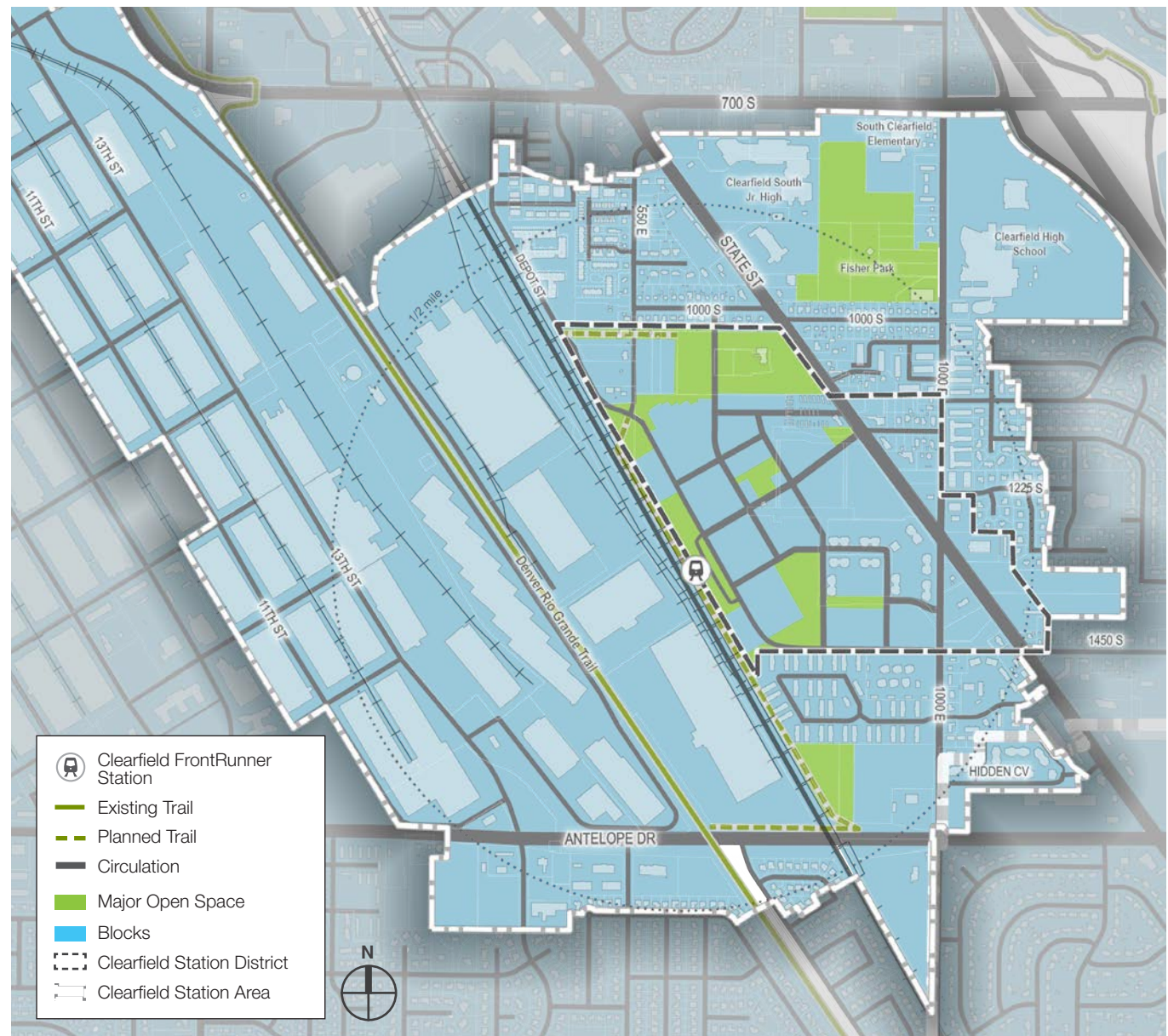
The Layton Town Center lies just beyond the half-mile zone of influence of the station, with Holy Cross Hospital -Davis and well-established residential neighborhoods just beyond. A strong connection between the Station Area and the town center will increase transportation, office, retail, commercial and residential opportunities.



Framework Plan: Streets, Blocks & Open Spaces

The Framework Plan for the Clearfield Station Area shows the defining features of the planning area, including the circulation system, block patterns, and open spaces. The physical arrangement of the streets and blocks establishes the form of the area, the framework for the Station Area, and its surroundings.

The Framework Plan highlights the key elements of the Station Area and how they are aligned and coordinated. Merging a connected street network with appropriately sized blocks and an integrated open space system is critical for ensuring the vision for the Station Area is realized.



Future Land-Use

The Clearfield Station Area is a diverse neighborhood that contains a variety of land-uses within the Station Area and its zone of influence. When complete, the area will merge existing neighborhoods and uses with new ones, resulting in a complex mix of complementary uses. These can be developed as horizontal mixed use projects (a variety of single use buildings) or vertical mixed use projects (multiple uses within individual buildings).

The accompanying land-use diagram details and refines the envisioned land uses for the area. The Clearfield Station District is where the bulk of new development and change is envisioned and is the focus of many of the subsequent sections of this document.

Future land-uses are arranged with the highest intensity uses concentrated near the center of the MDP site adjacent to the platform and are assumed to generate high transit ridership.

Table 1 indicates the anticipated areas and percentages of land allocated to each use.

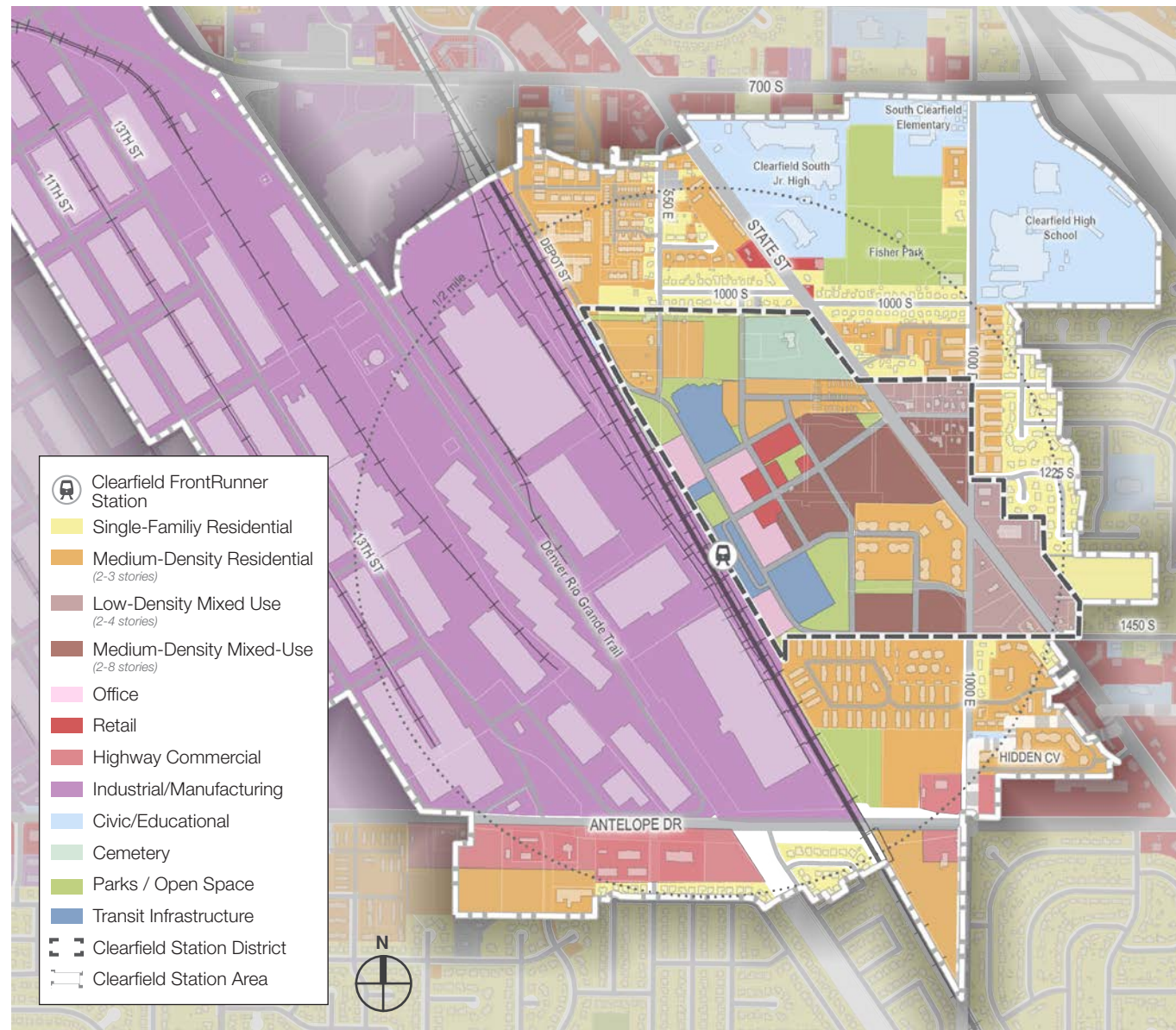


TABLE 1: FUTURE LAND USE ACREAGES

Name	Station District		Station Area Outside Station District		Total Station Area	
	Acres	Percent	Acres	Percent	Acres	Percent
Single-Family Residential	0.0	0%	42.7	6%	42.7	5%
Medium-Density Residential	21.9	23%	87.6	12%	109.5	13%
Low-Density Mixed-Use	14.3	15%	0.0	0%	14.3	2%
Medium-Density Mixed-Use	14.8	16%	0.0	0%	14.8	2%
Office	5.5	6%	0.0	0%	5.5	1%
Retail	5.6	6%	0.0	0%	5.6	1%
Highway Commercial	0.0	0%	19.0	3%	19.0	2%
Industrial/Manufacturing	0.0	0%	533.7	71%	533.7	63%
Government/Institutional	0.0	0%	48.7	6%	48.7	6%
Cemetery	6.5	7%	0.0	0%	6.5	1%
Park/Open Space	17.1	18%	21.1	3%	38.2	5%
Transit Infrastructure	9.6	10%	0.0	0%	9.6	1%
Total	95.3	100%	752.8	100%	848.1	100%

SINGLE-FAMILY RESIDENTIAL

Existing single-family residential neighborhoods should be maintained and incorporated into the structure of the Clearfield Station Area. A limited amount of new single-family residences may be warranted to help improve transitions with other uses in the area.



MEDIUM-DENSITY RESIDENTIAL

These areas include a mix of townhome, duplex, and/or multi-plex units that provide “Missing Middle” housing opportunities within convenient walking distance to the commuter rail station. Heights should generally be limited to three stories.



LOW-DENSITY MIXED-USE

These areas provide a mix of lower-density housing options including multi-plexes and small apartment buildings, from two to four stories in height. Ground floor uses are envisioned to include a mix of residential, office, and retail uses.



MEDIUM-DENSITY MIXED USE

Primarily concentrated around the intersection of Station Boulevard and State Street, these areas provide medium-density mixed-use buildings between two and eight stories in height. Ground floor uses are envisioned to include a mix of retail, office, entertainment, restaurant, general commercial and residential amenity spaces. The highly visible location will provide retail services for both the Clearfield Station Area and traffic on State Street. Housing and/or office uses are encouraged over the retail ground floor.



OFFICE

The office zone accommodates office buildings in the heart of the neighborhood, directly adjacent to the commuter rail platform. The central location of this use will help establish the identity of the neighborhood as not just a residential community, but a complete community centered around an employment hub. The central location of this zone requires some active ground floor commercial uses in prominent areas.



RETAIL

The retail zone provides a retail element near the station. This highly visible location will provide retail services for both the Clearfield Station Area as well as vehicular traffic from State Street. Housing and/or office uses are also possible, with retail limited to the ground floor.



HIGHWAY COMMERCIAL

These areas provide highway-oriented retail opportunities along Antelope Drive and the intersection with 1000 East. These are high visibility locations that will provide retail services for motorists operating in the vicinity of the area, including vehicular traffic from State Street. Carefully-incorporated residential and office uses are encouraged on the upper floors.



INDUSTRIAL/ MANUFACTURING

Freeport Center uses are anticipated to grow and evolve over time, bringing greater numbers of employees to the area. To help ensure the center takes advantage of the transit, retail, office, and entertainment opportunities with the Station Area, vehicular, microtransit, pedestrian, and cycling linkages should be considered as part of any future redevelopment in the Freeport Center.



CIVIC/EDUCATIONAL

The area is well served by three K-12 public schools and a public park north of the Station Area. These facilities should be preserved and enhanced to meet the needs of the Station Area and the Clearfield community as a whole.



PARK / OPEN SPACE

A range of new parks, plazas, greenways, and streetscapes are proposed to establish the Station Area as a robust and engaging city center. These uses should be mixed with retail shops and other public amenities to help facilitate the creation of a gateway experience into this new and dynamic district. These efforts should be combined with upgrading efforts for Fisher Park and other existing parks on the periphery of the planning area, to help ensure a high level of park and open space opportunities are available to serve the expanded population in the area. The public space zone contains the neighborhood's significant public open spaces, including recreational and functional open spaces. The plan shows the existing drainage basin, as well as a central location for a village square.



CEMETERY

The existing cemetery will be retained, with pedestrian access integrated into the park and open space network.



TRANSIT INFRASTRUCTURE

The transit infrastructure within the Station Area provides transit users with central, comfortable, safe, and convenient infrastructure that accommodates all modes of transit. A transit plaza will provide civic space, as well as amenities that enhance the overall transit user experience. This includes small buildings and kiosks for food and beverage, bike rentals and micromobility, ticket stations, and other amenities geared toward transit riders. Transit uses are served by parking locations within 1,000 feet of the commuter rail platform to ensure an appropriate amount of parking is available for park-and-ride transit users. Parking in this area can also act as shared parking for employees and visitors in the neighborhood.



Station District Illustrative Master Plan

This section focuses on the **Station District**, as it contains the majority of proposed redevelopment (see Future Land Use on page 37).

The Illustrative Master Plan presents an example layout of how the Clearfield Station District could develop to meet the vision and principles established for the project. As previously described, this district area represents the most development-ready zone within the Station Area. The other districts are well-established and expected to generally remain within their current form.

The building sizes, shapes, and uses shown here are flexible and are intended to demonstrate the vision for the development. The layout and arrangement of the buildings is also flexible.



ILLUSTRATIVE RENDERINGS

Concept renderings demonstrate the general character and feeling of the Clearfield Station Area. They are meant to illustrate the general vision, not specific design solutions.

The images on this page provide two views along Station Boulevard looking toward the FrontRunner Station – one viewed from above the street, and the other from a street level perspective.

ILLUSTRATIVE RENDERING: STATION BOULEVARD LOOKING WEST

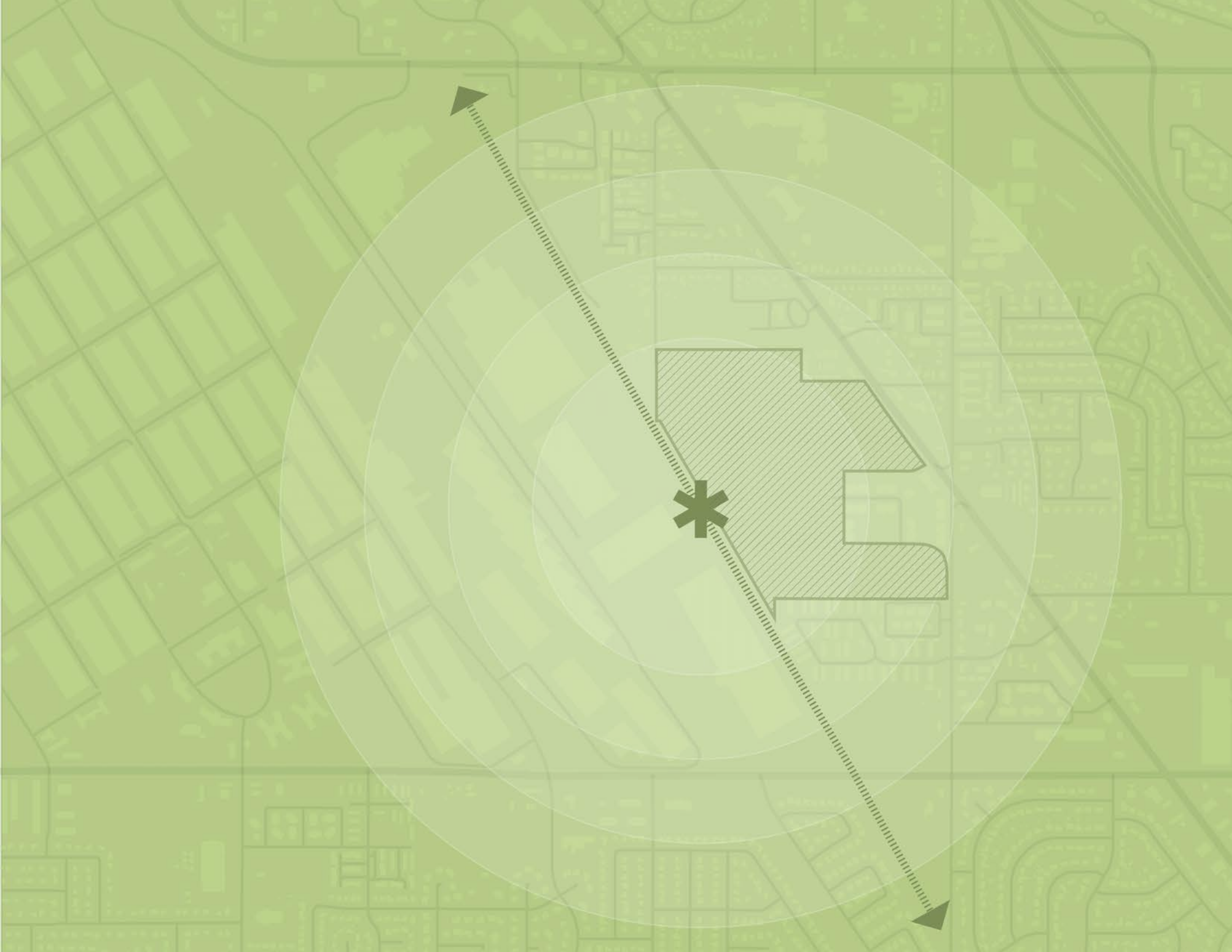


Rendering Credit: IBI Group

ILLUSTRATIVE RENDERING: STATION BOULEVARD LOOKING WEST



Rendering Credit: IBI Group





04

MARKET STUDY + ECONOMICS

Market Study + Economics

INTRODUCTION

With the proposed development within the Clearfield Station Area Plan, there is tremendous potential for Clearfield City (City) to generate increased revenues. This analysis calculates the possible revenue generation for the land use types, factoring in the City’s main General Fund revenue sources: Property Tax, Sales Tax, Municipal Energy Tax, and Class B/C Road Funds.

ASSESSED VALUES IN DAVIS COUNTY

For the most accurate revenue projections, average assessed values were calculated for different development types that are found within the Station Area plan. These are based on similar properties throughout Davis County, according to 2023 values provided by the County.

TABLE 2: AVERAGE DAVIS COUNTY ASSESSED VALUES

Land Use Category	Average Assessed Value
Office	\$186.84/SF
Mixed-Use	\$198.67/SF
Retail	\$135.82/SF
Multi-Family	\$237.72/SF
Single Family	\$186.87/SF

Source: Davis County Assessor's Office



POTENTIAL REVENUE GENERATION

Table 3 demonstrates the approximate acreage and total revenue generation of each major land use type within the Station District. This analysis does not include the entire Station Area, focusing in on just the Station District where the majority of land use change is proposed (see Districts Map on page 33 and Future Land Use Map on page 37).

The mixed-use development is planned to contain both residential and commercial uses, in a primarily vertically stacked configuration. There are additional uses proposed within the Station District, but they are primarily non-revenue generating properties such as open spaces, parking garages/ areas, and transit zones.

These calculations represent additional revenues the City may collect as the project is developed. The City will continue to receive revenue from other areas within the Station Area boundaries, however they are not reflected in these calculations.

Total revenues shown demonstrate an aggregated total of major General Fund revenue sources for the City: Property Tax, Sales Tax, Municipal Energy Tax, and Class B/C Road Funds. The revenue projections may vary from what is actually collected, depending on what is developed. For example, the mixed-

TABLE 3: STATION DISTRICT DEVELOPMENT REVENUE GENERATION

Land Use Category	Approximate Acres	Total Revenue (Mixed-Use with Office)	Total Revenue (Mixed-Use with Retail)
Stand-Alone Retail	15.9	\$398,857	\$398,857
Retail	5.6	\$189,055	\$189,055
Mixed-Use	28.0	\$991,638	\$1,373,441
Office	5.5	\$190,135	\$190,135
Multi-Family Residential	22.9	\$585,004	\$585,004
Low-Medium Density Residential	3.5	\$52,017	\$52,017
Total	81.4	\$2,406,706	\$2,788,509

use development may include office, retail, or some combination of the two, along with multi-family residential. As Table 3 demonstrates, the total revenue collected varies depending on what use is found within the mixed-use area.

The development type with the greatest revenue generating potential is mixed-use with a focus on retail as the commercial development. This is in large part thanks to the impact of sales tax. The increased taxable sales

projected for these businesses results in more potential revenue generation. It is interesting to note that multi-family residential development has the second greatest revenue. With the rise in online shopping, homes have become miniature retail stores, with cities able to collect point of sale revenue from these sales. With more dense residential developments, this increases the revenue collection.

Funding Sources

The City has a number of avenues at its disposal to help incentivize development in this area, or to help offset development costs. The following table summarizes a number of these different funding opportunities.

TABLE 4: AVAILABLE FUNDING MECHANISMS

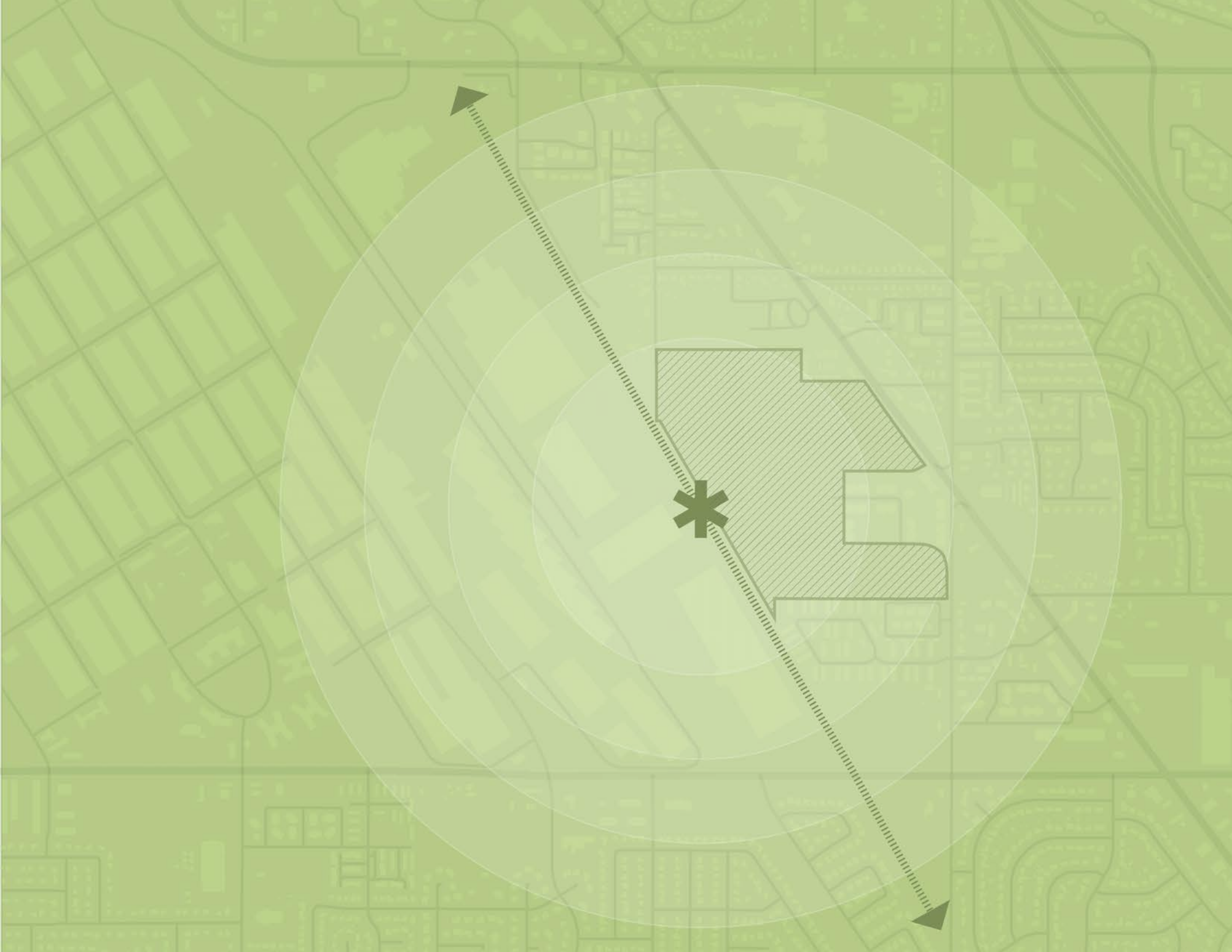
Funding Source	Advantages	Disadvantages
Tax Increment Financing (Community Reinvestment Area – CRA)	Taxes generated in an area are spent in same project area; Potential participation by other taxing entities; Can include specialized TIF areas such as HTRZs and TRZs	Must get approval of other taxing entities – subject to political will
Bonding (General Obligation GO, Sales Tax)	GO bonds have the lowest rates; Sales tax bonds do not require public approval/ vote; Funds are available immediately	GO bonds require public vote
Utility Bond	Immediate funding; No public vote required	Rates may need to be raised to cover utility costs; Used only for utilities
Impact Fees	New development pays its own way – proportionate share of capital costs; Could create separate service area for separate impact fees if extraordinary costs apply; Could be a long-term repayment source for other funding mechanisms	Receipt of impact fees takes place over many years and is not guaranteed; Not every project is impact fee eligible
Public Infrastructure District	Off the City's books; Those who benefit pay; Cost is much lower than other development financing; Used instead of impact fees and is a steady stream of revenue	Willingness of all property owners to establish a PID; Ongoing PID governance; Competitiveness of site with additional taxes
Special Assessment Area	Those who benefit pay; Could be used in conjunction with tax increment, thereby encouraging development and use of increment to pay assessments	Willingness of property owners to establish a SAA – requires 60 percent or more to agree (based on assessment method); Need to come up with equitable assessment method
Public-Private Partnerships	New revenue stream that pays for infrastructure	Relatively untried; Would lose control of rates to private investor
Grants	Additional money that does not come from the City; Ability to enhance funds already committed to projects	Funds are subject to availability from the granting institution; often times requires matches or other restrictions

Economic Incentives

HOW COULD POTENTIAL USES BECOME MORE FEASIBLE AT CLEARFIELD STATION?

- **Opportunity Zone** – This area falls in a designated Opportunity Zone. This is a major investment incentive that creates a superior advantage to most other Frontrunner Stations.
 - Significantly increases investment appeal and makes office and retail more financially feasible (investors will accept lower capitalization rates (creating higher values) due to the tax advantages).
- **Funding Incentives** – The area is part of an existing CDA. Available funding incentives should be readily marketed to attract uses the city desires.
 - Additionally, the city and UTA should consider the formation of a Transportation Reinvestment Zone (TRZ), a newly adopted economic development tool that focuses on tax increment financing for transportation specific improvements. This funding option, while very similar to an RDA/ CRA, does not require a ten percent allotment to affordable housing. It also allows for the land owner and city to have greater control regarding what can be built.
- **Increase Daytime Population** – an increase in daytime population will benefit retailers. This can be accomplished by the following:
 - Entertainment draw/attraction
 - Strong office population
 - Strong residential population (to capture remote workers)







05

BUILDINGS + ARCHITECTURE

DESIGN GUIDELINES

Buildings + Architecture

OVERVIEW

The layout and arrangement of buildings and parking typically have the most significant impact in creating a walkable destination. The arrangement of buildings and parking reinforces the quality and functionality of the building facades, streets, and open spaces and how all of these elements work together to create a more livable environment.

The following guidelines are meant to apply to the Station District (see District Map on page 33) and other mixed-use development areas within the Station Zone of Influence.

INTENT

To establish strong urban design guidelines for the Station District and other mixed-use development areas within the station Zone of Influence that will serve as the foundation to thoughtfully choreographing buildings, open space and streets.

PRIMARY FACADES

Primary facades establish a consistent streetwall with active ground floor uses. As illustrated on the map on the following page, they often line primary streets – the most important and walkable streets in the neighborhood. Primary facades should address the street with windows/transparency, high quality building materials, and a main building entrance.

Retail, residential, and/or other active uses are encouraged where a building faces a primary street.

SECONDARY FACADES

Secondary facades should be used when a building fronts multiple streets. The secondary facades should include windows/transparency and high quality building materials. However, such treatments are not as essential as they are on primary streets. Retail, residential and/or other active uses are encouraged. Blank walls should be limited.

PARKING

Parking areas should be located in the rear and to the sides of buildings, and should not face the Primary streets.

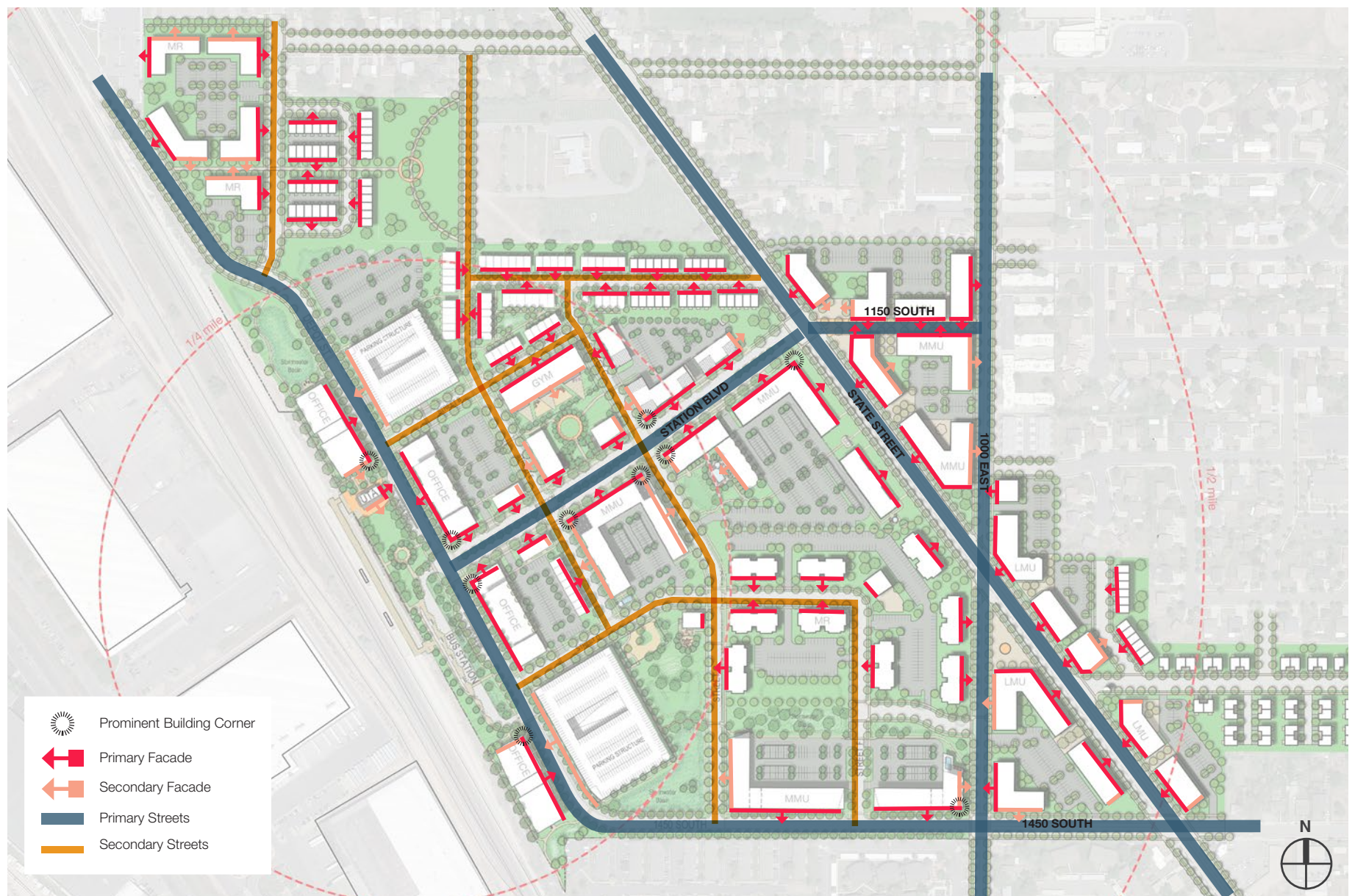
Buildings should wrap and screen parking areas from the street where possible and/or applicable.

OPEN SPACE

Open spaces should be located throughout the Station Area in prominent locations and include various sizes and user experiences.

Open space design and programming should respond to the surrounding uses and buildings.

See *Chapter 6: Open Space + Public Realm* for details.



Architectural Style

INTENT

To establish a specific “look and feel” throughout the study area to unify the area and create a design theme that is appropriate for the Clearfield Station Area.

DESIGN THEME - “CONTEMPORARY INDUSTRIAL”

The design theme for the Station Area is contemporary industrial style that is modern, yet is rooted in the industrial character of its surroundings. This industrial character helps to create a brand for the area and provides a common theme that ties the neighborhood together.

There are no historic buildings on or directly adjacent to the MDP site. Therefore, this presents an opportunity to create a new and unique, industrial inspired architectural style.

The design guidelines section will provide detailed design guidelines that should be followed to achieve a consistent and coherent architectural style as outlined above.

HISTORICAL PRECEDENTS

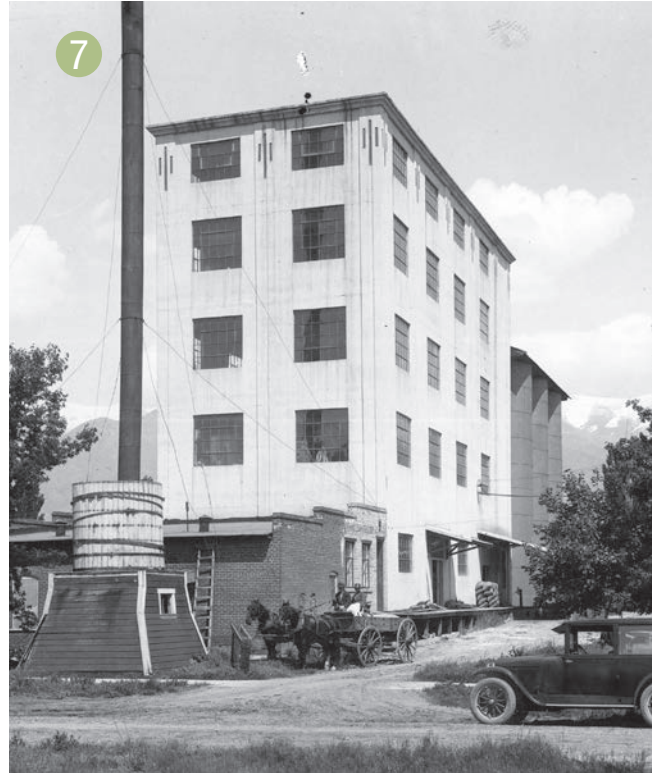
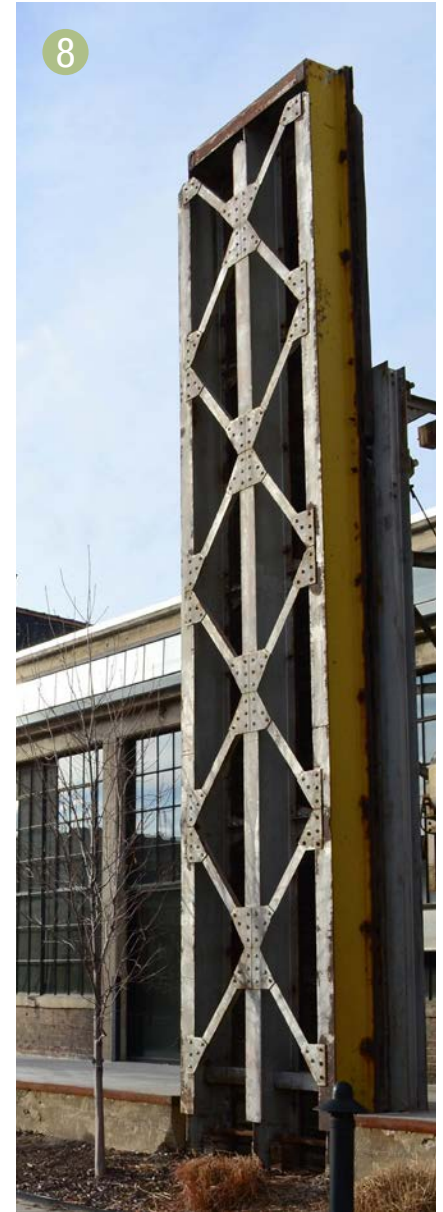
There are no historic buildings currently existing in the area, and therefore, historic precedents should be considered from around Northern Utah. Precedents should be based on traditional industrial architecture from the early to mid 20th Century that are/ were found in Northern Utah.

The images to the right display buildings found in Clearfield, as well as nearby cities such as Ogden, Layton, and Kaysville. These are just a few examples of existing and former buildings from the area that should provide inspiration for architects and designers.

PRECEDENT IMAGES

- 1 Administration building at the Clearfield Naval Supply Depot (now Freeport Center)
- 2 Layton Sugar Company
- 3 American Can Company (Ogden)
- 4 DaVinci Academy (Ogden)
- 5 Pillsbury Company (Ogden)
- 6 Warehouse (Ogden)
- 7 Kaysville Flour Mill
- 8 American Can Company (Ogden)





Architectural Style

CONTEMPORARY PRECEDENTS

The buildings on the following pages demonstrate images found throughout the country that achieve the goal of creating a contemporary, modern building that is also rooted in historic industrial architecture. They reflect the character and level of detailing envisioned for the Clearfield Station Area.

The images illustrate a range of precedents, from more abstract interpretations, to more traditional recreations. These images should be used for reference and inspiration for new development on the Clearfield Station Area.

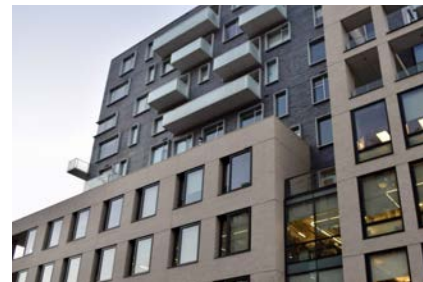
Elements often associated with industrial architecture include, but are not limited to:

- Large volumes that house large-scale industrial activities such as a mill, factory, foundry, refinery or power plant.
- Predominantly brick and steel buildings.
- Specialized building elements and apparatus such as tall chimney

stacks, exposed materials circulation apparatus, hoists and chutes.

- Exposed structural elements.
- High interior spaces with exposed brick, steel and timber.
- Divided light windows.





Materials + Colors

INTENT

To ensure a consistent application of complementary and high quality materials throughout the neighborhood that will reinforce the unique identity and a sense of place.

DESIGN GUIDELINES

- Building materials should reinforce the industrial theme by using brick, steel, timber, and concrete.
- Building materials should be durable, high quality, and authentic materials that have a long life, age well, and reflect a high level of craftsmanship.
- Building materials should add texture, depth, and visual interest to the building's facade.
- Materials should turn corners and incorporate thoughtful transitions between facades, spaces, uses, and structures.
- Materials should generally be limited to one or two predominant materials and one or two accent materials in order to keep buildings visually coherent and uncluttered.
- EIFS stucco and corrugated steel should be limited to no greater than 30% of the building's facade.

COLOR

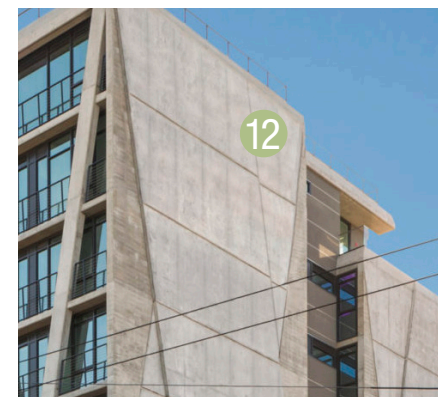
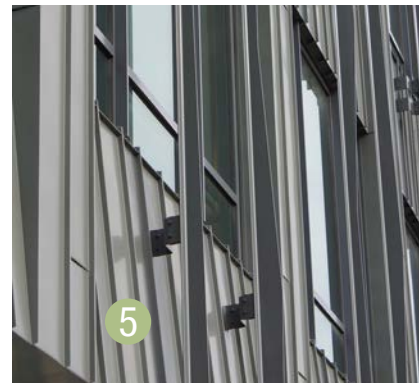
Industrial buildings typically are defined by dark, heavy colors, such as red brick, black steel and dark concrete.

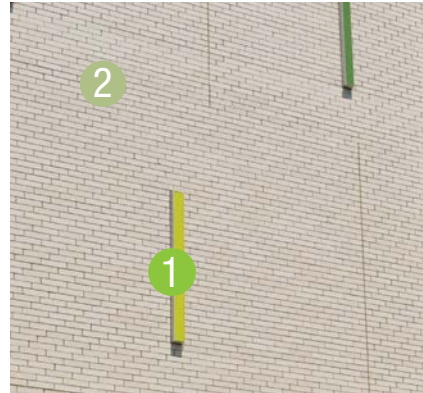
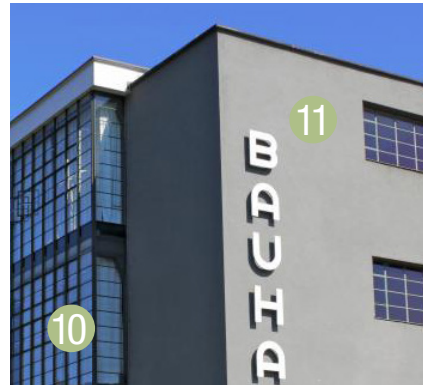
While those colors and materials are appropriate, lighter colors are highly encouraged in order to give the district a more fresh, contemporary look. Pops of color are also encouraged to accent and bring a feeling of excitement and uniqueness to the neighborhood.

1 Pop of Color as an Accent

ACCEPTABLE MATERIALS

- 2 Brick
- 3 Tumbled Brick
- 4 Black Steel
- 5 Colored Pre-Finished Metal Panels
- 6 Corrugated or Corten Steel
- 7 Stone
- 8 Wood / Timber
- 9 Curtain Walls Glazing System
- 10 Industrial Sash / Divided Light Windows
- 11 EIFS Stucco
- 12 Concrete





Architectural Massing

INTENT

To facilitate building shapes that fit comfortably within their surroundings, are friendly and unobtrusive to pedestrians, achieve an attractive urban form, and are visually interesting.

DESIGN GUIDELINES

- The most dense uses and tallest building heights should be located in Medium-Density Mixed-Use areas (see Future Land Use Map on page 35).
- Buildings should be designed to a human scale, with particular attention on the ground floor
- Floorplates should generally be less than 30,000 sf per building, with no minimum floor plate size.
- Buildings should create a consistent streetwall on both sides of the street to create “enclosure.”
- Gaps in the streetwall should be limited as much as possible.

PRECEDENTS

- 1 Building has clearly defined top, middle, and base.
- 2 Multiple buildings combine to create a good, pedestrian-scaled streetwall. The buildings also demonstrate a clearly defined top, middle, and base.





ARCHITECTURAL MASSING

Architectural massing is key in creating an inviting pedestrian environment. Care should be taken to understand the form of buildings and their impact on the public realm.

This graphic demonstrates how careful architectural massing creates an interesting and pedestrian friendly urban environment.

- 1 A consistent streetwall on both sides of street, as well as vertical elements such as trees, create a sense of enclosure.
- 2 A variety in building height, scale and bulk creates a dynamic and visually interesting experience.
- 3 Buildings include setbacks on upper stories in the building facade to ensure pedestrian scale and increase sunlight and air on the street.
- 4 The ground floor of buildings addresses the street and has a high level of transparency.
- 5 Windows, podium decks and balconies overlook the street.

Facade Articulation

INTENT

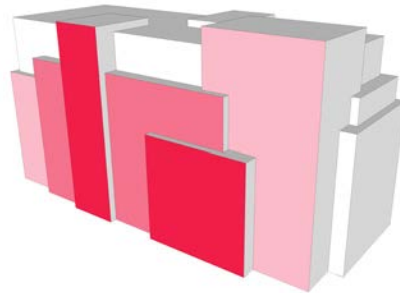
To purposefully articulate building facades in order to make the various building functions legible through the massing of the buildings, as well as to reduce the building's apparent mass.

HORIZONTAL ARTICULATION

The first 20 feet of height of building faces should have a rhythm of modules that serve to break down the scale of the building face. A module is defined as a portion of the facade that is differentiated from the adjacent facade by a change in the line of the face of the building, and/or a substantial change in material color or fenestration. Characteristics between modules should relate to one another to achieve a unified composition.

DESIGN GUIDELINES

- Modules should generally be no longer than 40 feet.
- Building facades should avoid being long, monotonous, and repetitive.
- Articulation should be used to create interest and help establish a strong sense of design and identity.
- Massing, building details, and entries should be proportionately scaled.



Vertical planes are articulated through massing and add interest to the building



VERTICAL ARTICULATION

The three segments of the building - the base, middle and top - should be articulated by such elements as cornices, string courses, stepbacks, recesses and projections, changes in floor height, and changes in color and material.

DESIGN GUIDELINES

Top Section

- Should define the roof line.
- Stepbacks are encouraged for penthouse units or to otherwise break up the mass and define the building top.
- Incorporate green roofs and other usable roof space where possible.

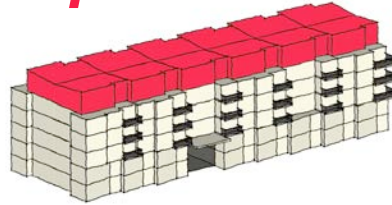
Middle Section

- Should define the principle building facade.
- Should differentiate from the base and top sections through the use of massing, materials, and/or color.

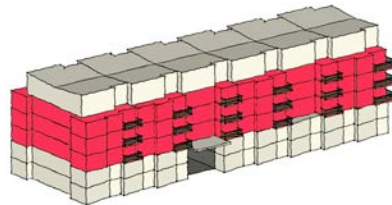
Base Section

- Should relate directly with the street.
- Should “ground” the building.

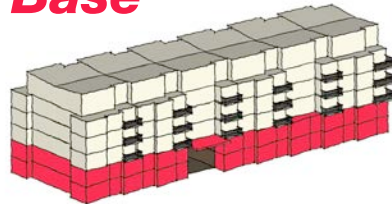
Top



Middle



Base



Setbacks

INTENT

To ensure all buildings consider their relationship with the public right-of-way with the appropriate setback distance for each unique use, and to create a human-scaled, defined streetwall.

DEFINITION

The setback refers to the space between the building facade and the public right-of-way line.

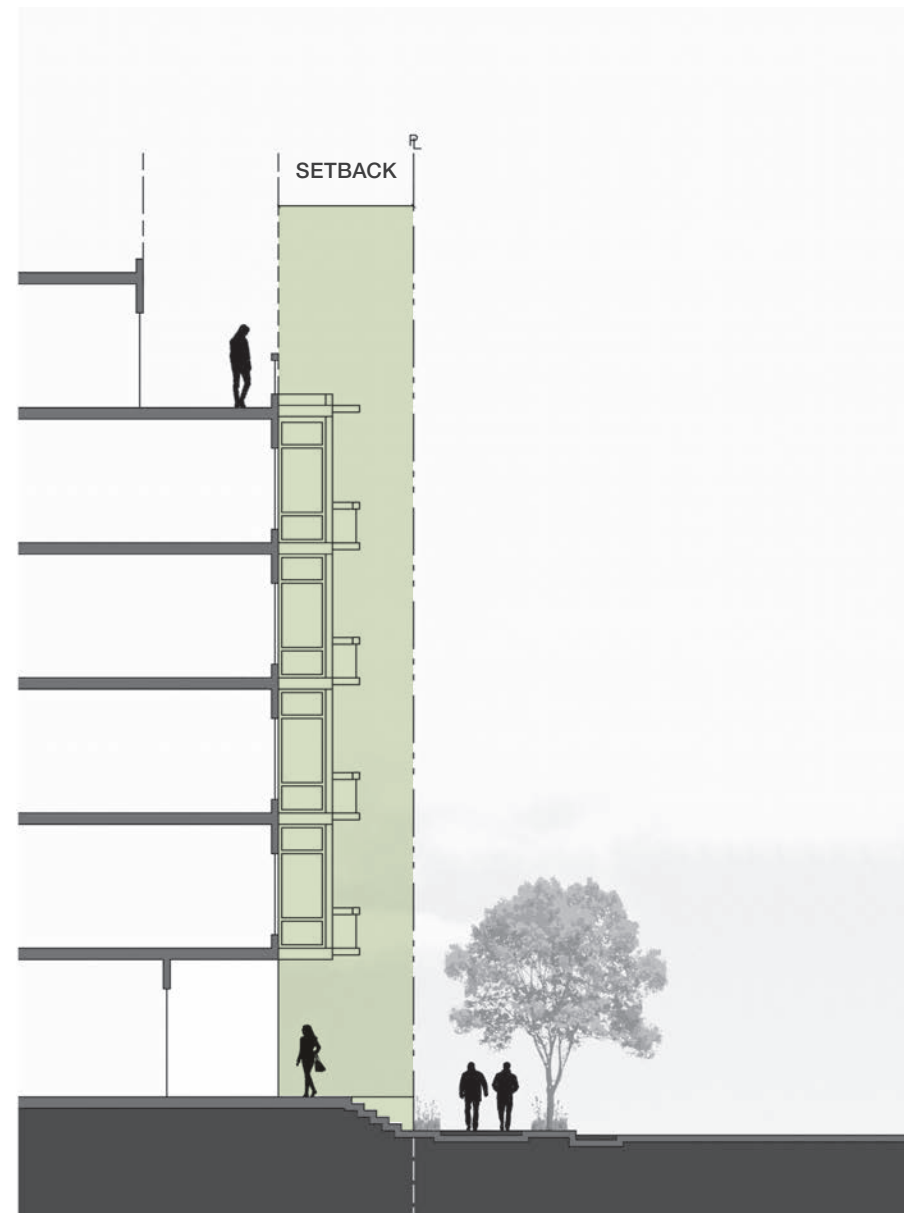
DESIGN GUIDELINES

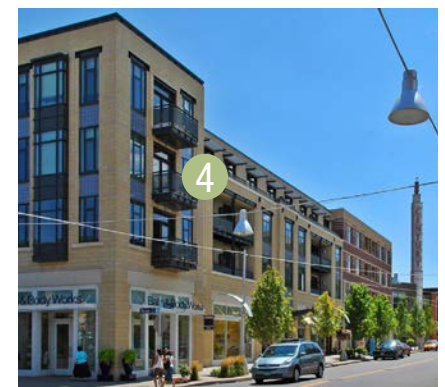
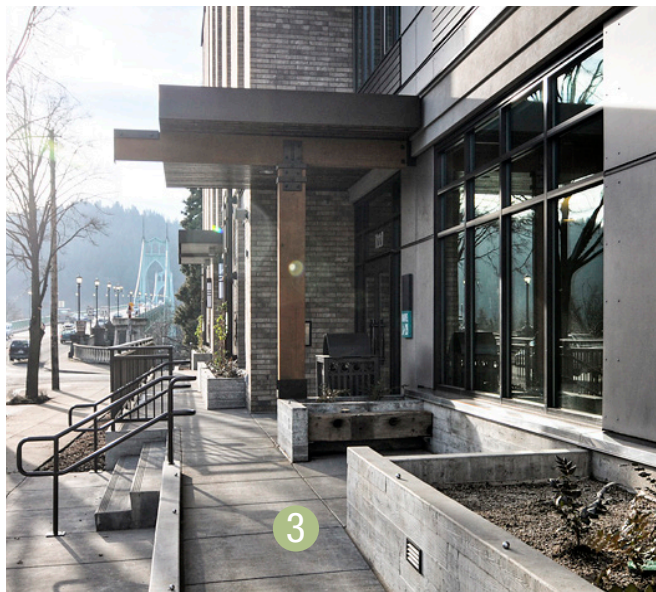
- Maximum setback distance is 15 feet unless a building fronts a plaza or open space.
- There is no minimum setback distance.
- Generally, setbacks should be no more than 5 feet.
- Setbacks, when used, should enhance the ground level environment and pedestrian experience. Examples include:
 - To create a space for outdoor dining in front of retail/restaurant spaces.
 - To provide landscape and/or a patio/stoop in front of ground level residential entrances.

- To enhance the architectural character of the building facade at street level.
- Entrance courts for office or residential building lobbies.
- To add interest and bring nature into the streetscape through planters and landscape. In-ground planters are only allowed in front of ground-floor residential units.
- Setback may be raised above sidewalk level to create feeling of semi-private space.
- See pages 68 - 61 for ground floor - base activation design guidelines.

PRECEDENTS

- 1 Setback is used for outdoor dining.
- 2 Setback along ground floor residential units contains stoops and landscape.
- 3 Setback is raised to create sense of semi-private space.
- 4 A strongly defined streetwall is created, despite having some setbacks in the building face and at the ground floor.





Projections

INTENT

To encourage facade articulation through habitable and non-habitable projections.

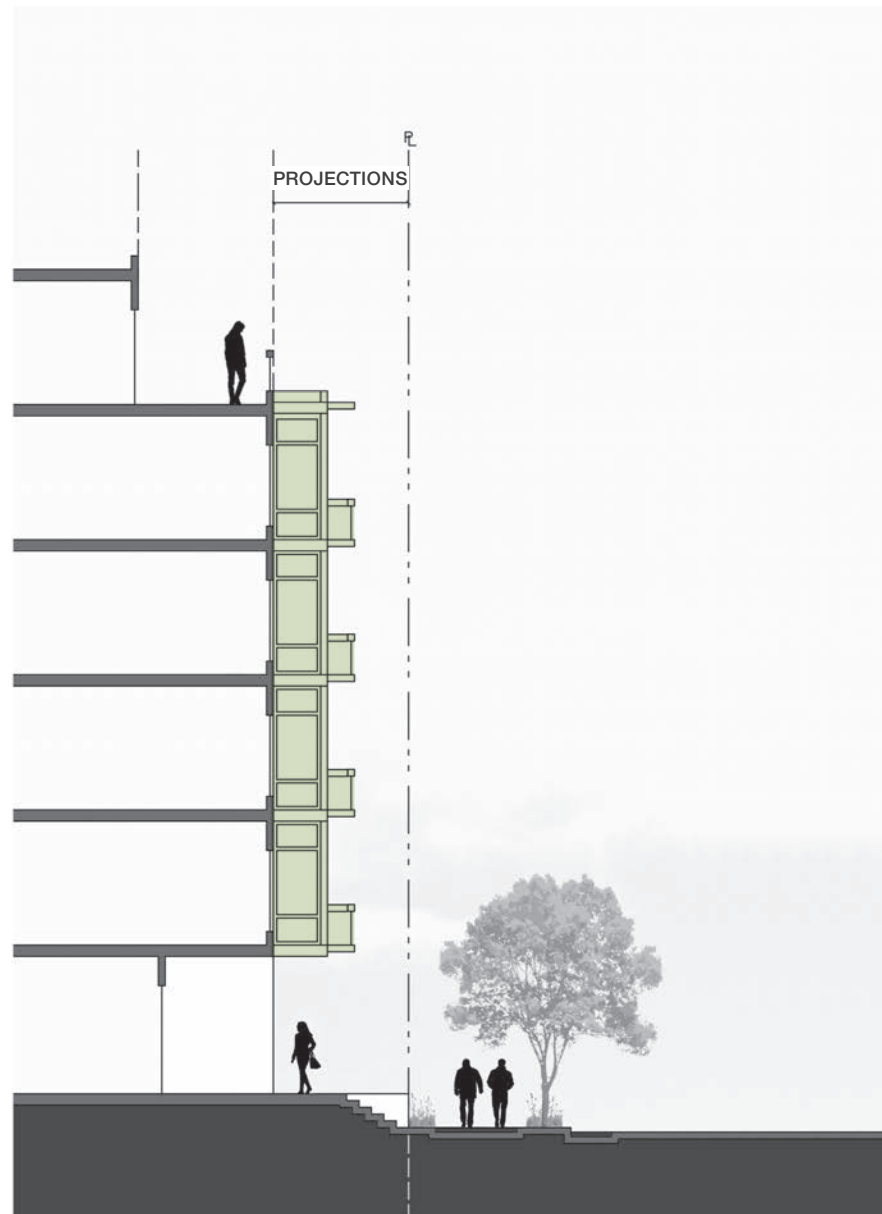
DEFINITION

Habitable projection - a portion of the building enclosed by walls and a roof, such as a bay window, corner element, or other extended bay.

Non-Habitable projection - spaces utilized by residents but not enclosed by walls and a roof, such as balconies.

DESIGN GUIDELINES

- Projections are encouraged to add visual interest to the facade, as well as to add usable balconies as residential amenities.
- Balconies should be at least 3 feet deep.
- Projections should not extend more than 6 feet into setback or common space.
- Projections should not extend more than 3 feet into public right-of-way.
- Decorative elements such as belt courses, cornices, sills and eaves are also encouraged.



Stepback

INTENT

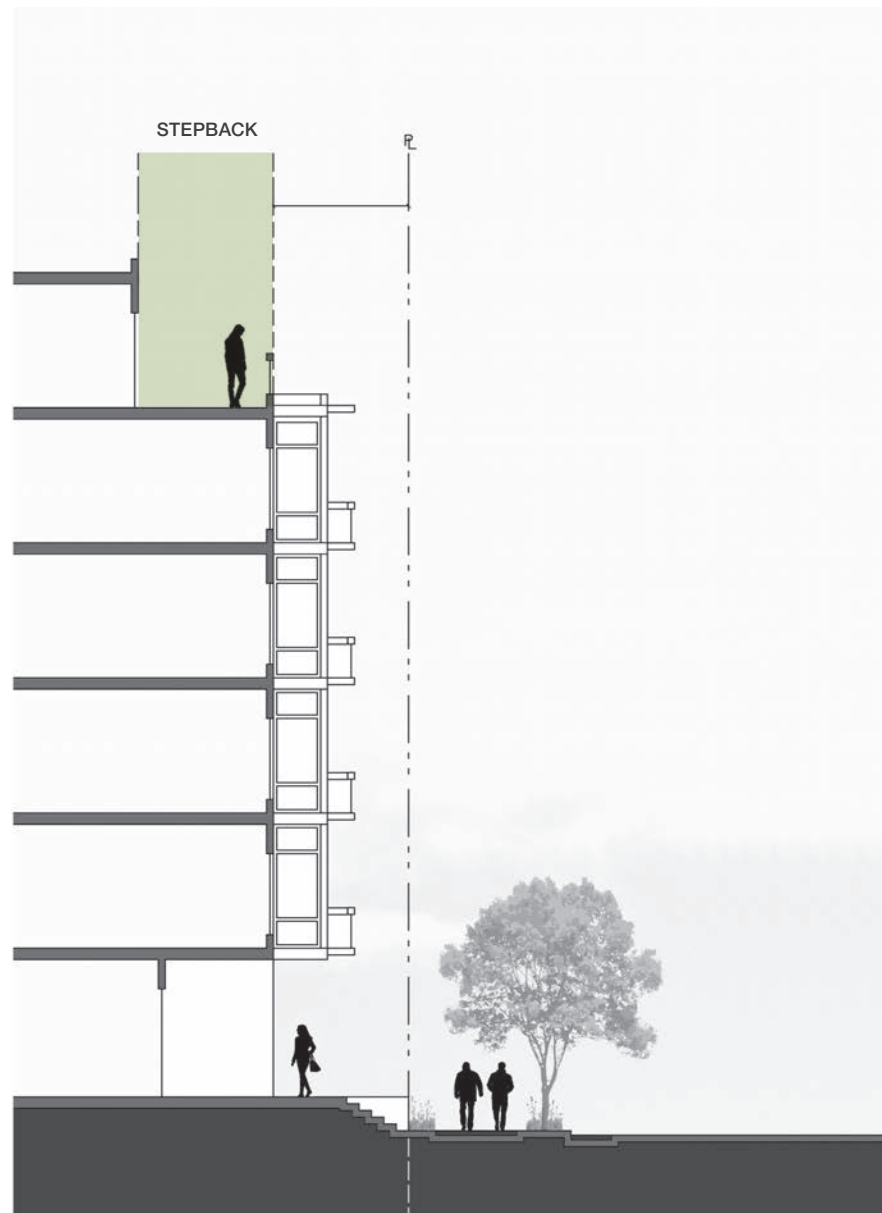
To encourage facade articulation and the creation of usable outdoor space by offsetting the upper floor(s) from the lower floor(s) of a building.

DEFINITION

Stepback is the portion of the building on upper levels that is stepped back from the building facade.

DESIGN GUIDELINES

- Stepbacks are encouraged to help break down the mass of the building by creating a defined “top,” as well as to add usable space for residential amenities.
- Roof space created by stepbacks should be designed as usable outdoor space.



Ground Floor - Base Activation

INTENT

To ensure the important interaction between the ground floor of a building and the sidewalk is carefully designed to enhance the pedestrian experience and the overall vitality of the neighborhood.

OVERVIEW

One of the most important aspects of a walkable urban neighborhood is the street level interaction between the building and the street. For a streetscape to facilitate active public life, it is essential that buildings address the street on the ground floor.

This page contains general ground floor design guidelines, while the following pages contain specific guidelines for residential and commercial uses.

DESIGN GUIDELINES

- The base of the building should be designed to foster positive activity by orienting and integrating courts, lobbies, entries, and large windows to face streets, public parks, and open spaces to provide more opportunity for interaction and safety.

- Avoid or minimize expansive blank walls at the ground floor.
- Include operable windows, roll up doors, and other features to activate and animate a building.
- Maximize transparency of ground floor commercial facades with windows and doors with visibility into active uses, such as retail spaces, lobbies, etc.
- Highlight entrances to commercial buildings through integrated signage, changes in materials and colors, and/or through changes to the buildings massing.
- Ground Floor heights should be at least 14 feet tall.
- Active uses should have a depth of at least 25 feet from the street frontage.

PRIMARY STREETS

The primary streets, as defined in the Street Hierarchy Section on page 97, are the most important streets where active ground floor uses should address the street. “Primary Street A” (the boulevard) is designed to be the primary retail and walking street in the neighborhood.

“Primary Street B” should also have active uses fronting the street. Retail is encouraged, if it is supported by the market. However, it is anticipated that this street will more likely be lined with active uses such as residential units, lobby spaces, meeting spaces, etc.

Active uses are encouraged on all other streets in the neighborhood to the extent feasible.

ACTIVE USES

Active uses are defined as any use that provides some level of interaction with the public realm. This could include uses such as residential, retail goods establishments, retail service establishments, public service portions of businesses, restaurants, taverns/ brewpubs, bar establishments, art galleries, theaters, performing art facilities and more. Uses must also be allowed by City Ordinance.

PARKING STRUCTURES

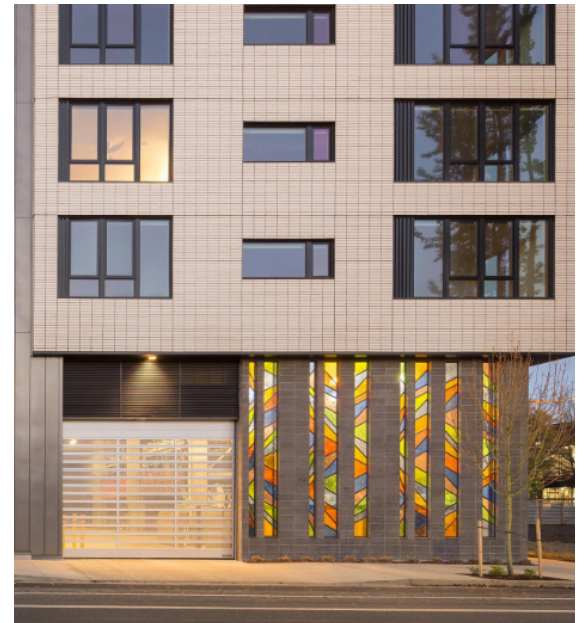
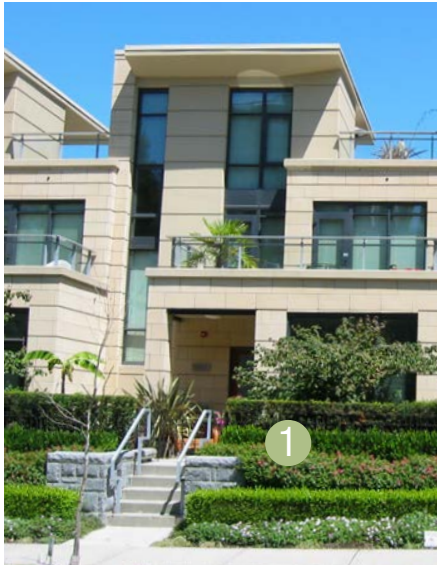
No parking structures are allowed to face “Primary Street A” and any parking structure facing “Primary Street B” should have an active ground floor use.

SCREENING METHODS FOR BLANK WALLS

Where blank walls occur, creative methods should be used to create interest on the streetscape. This could include solutions such as murals, green walls (plants growing on walls), faux windows, and more.

PRECEDENTS

- 1 Entrances at street level combined with high quality landscape buffer activates the street.
- 2 Storefront with high transparency on ground floor, along with outdoor dining, activates the street.
- 3 Roll up doors on ground level blend the indoor/outdoor space and activate the street.
- 4 Faux windows and landscape add visual interest to create feeling of activity on a facade without an active use.
- 5 Planters along blank street wall add interest to an otherwise blank wall.
- 6 Colorful glass adds interest and life to an otherwise blank wall.



Ground Floor Residential

INTENT

Residential buildings without retail or other active uses on the ground floor should activate the ground floor by putting residential units with individual entries that address the street on the ground floor.

GROUND FLOOR DESIGN ELEMENTS

1 LANDSCAPED SETBACK

Buildings with residential units on the ground floor should provide a setback, typically 10' or less, to provide space for entry steps/stoops and landscape in order to provide adequate space for the public/private transition. The landscape/plants should also be used to screen views from the street into residences (also see diagram on bottom right of this page).

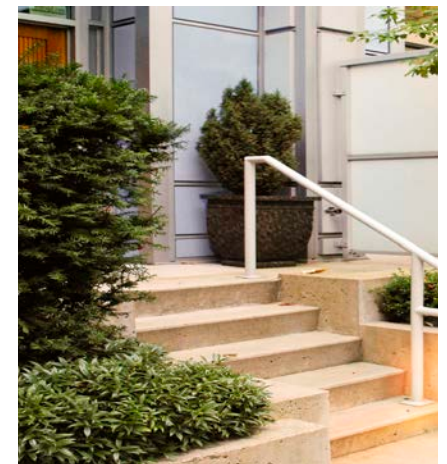
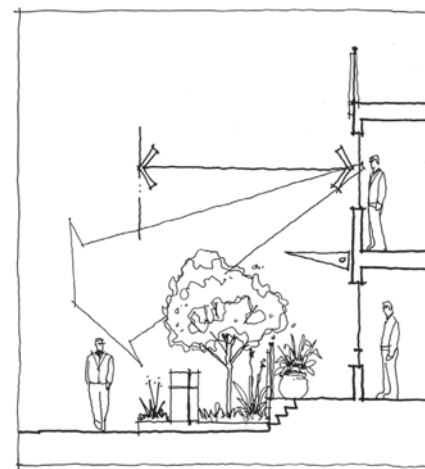
2 RESIDENTIAL ENTRY

Residential units on the ground level should generally be located at least three feet above grade, so that the unit's habitable space is above the eye level of pedestrians for increased privacy.



3 FACADE MODULATION

Buildings are vertically modulated at regular intervals of no greater than 30 feet to express individual ground floor residential units.



Ground Floor Commercial

INTENT

Commercial buildings should activate the ground floor through using retail or other active uses on the ground floor.

GROUND FLOOR DESIGN ELEMENTS

1 SETBACKS + LANDSCAPE

Commercial buildings should not have a consistent setback, but should have articulation zones as specified. Where setbacks do occur, landscaping is encouraged to soften the streetscape, add visual interest, and increase the opportunities for experiences with nature in an urban environment. Outdoor Dining or other functional uses that enhance the ground floor use are also encouraged

2 TRANSPARENCY

The ground floor of commercial buildings should be primarily composed of transparent materials in order to reveal activity of the building, as well as to add interest and security to the pedestrians.



3 FACADE MODULATION

Buildings are vertically modulated at intervals that align with the specific ground floor use, generally no greater than 80 feet. For retail uses, intervals should generally be no greater than 50 feet.



Roofs

INTENT

To emphasize the architectural style and to minimize visual impacts.

DESIGN GUIDELINES

- Roofs should be flat or appear flat from street level.
- Building heights and roof lines should modulate to create a visually appealing skyline and add interest to the skyline.
- Mechanical equipment on roofs should be screened from the street view.
- Green roofs are encouraged
- Usable roof terraces are encouraged
- Roofs should use high albedo, non-reflective materials to minimize heat island effect



Corners

INTENT

To emphasize important intersections and corners by including special architectural features on buildings in these key locations.

DESIGN GUIDELINES

- Incorporate special design details and architectural treatments that reinforce the corner's importance as a public realm element
- Corners in key locations should be emphasized by utilizing a combination of these measures:
 - A change in the building's massing and/or height
 - A contrasting facade finish
 - Transparency
- Designers/Architects are encouraged to find creative and artful solutions.



Entrances

INTENT

To emphasize the relationship between buildings and their adjacent streets by prominently featuring major entrances.

DESIGN GUIDELINES

- The main entrance to the building should provide the most important interaction between the pedestrian and building and should be emphasized through design.
- Buildings that front primary streets (as defined on page 97) should have a main entrance facing that street. A building may have an additional main entrance that faces the main parking area or drop-off zone, if applicable.
- Use lighting to highlight entrances.
- Provide canopies, awnings, or other overhead elements to protect users from weather conditions.
- The use of continuous “docks” within the build-to line is permitted to provide a semi-private space for outdoor dining or other uses that activate the streetscape. This mimics the re-purposing of loading docks that is often done on historic industrial buildings.



Fenestration

INTENT

To create a pedestrian friendly and engaging relationship between buildings and streets.

DESIGN GUIDELINES

- The ground floor of commercial buildings should have a high percentage of transparent materials where buildings front streets.
- Buildings maximize windows on upper floors that overlook streets or open spaces to increase “eyes on the street,” which discourages undesirable public behavior.
- Windows should be strategically used next to entrances and open spaces to create prominent indoor/outdoor relationships.
- Industrial windows are strongly encouraged to promote the industrial character.
- Mullions and frames are encouraged to project beyond the plane of the glass in windows to create strong shadow lines.



Building Signage

INTENT

To identify the commercial or non-commercial uses within the building with signage that promotes wayfinding, adds interest that fits with the architectural character of the building, and enhances the pedestrian experience.

DESIGN GUIDELINES

- All signs should be scaled appropriately to the size of the building.
- Signs shall be constructed of high quality and durable materials that are consistent with and complement the building materials.
- Building identification signage should be placed on facades that face the primary street(s).
- Signs should be artful and creative and work with a building's architecture to add interest.

RESTRICTIONS

Internally illuminated box signs with more than 30% of the internal area illuminated are not permitted.

Animated, blinking, or flashing signs are not permitted.

ACCEPTABLE SIGN TYPES

The following sign types are acceptable for attached building signs:

- 1 Wall signs** - Wall signs include signs that are attached to the face of a building wall. They should be mounted on the wall facing the public realm.
- 2 Window Signs** - Window signs are painted, placed, or affixed in or on the interior of a window, and intended to be viewed from the outside. Window signs should not obscure views into store or business.
- 3 Projecting Signs + Hanging Signs** - Projecting signs are attached to the building face and project out perpendicular to the building. Hanging signs are similar to projecting signs, except that they are suspended from a marquee or other overhead canopy.
- 4 Awning Signs** - Awning signs are signs that are mounted, printed on, painted on, or otherwise attached to an awning or canopy above a business door or window.
- 5 Mural** - Sign that is painted onto a wall that is visible to the public realm.



Building Lighting

INTENT

To integrate lighting on buildings into the architectural design to creatively illuminate pedestrian areas and highlight building elements.

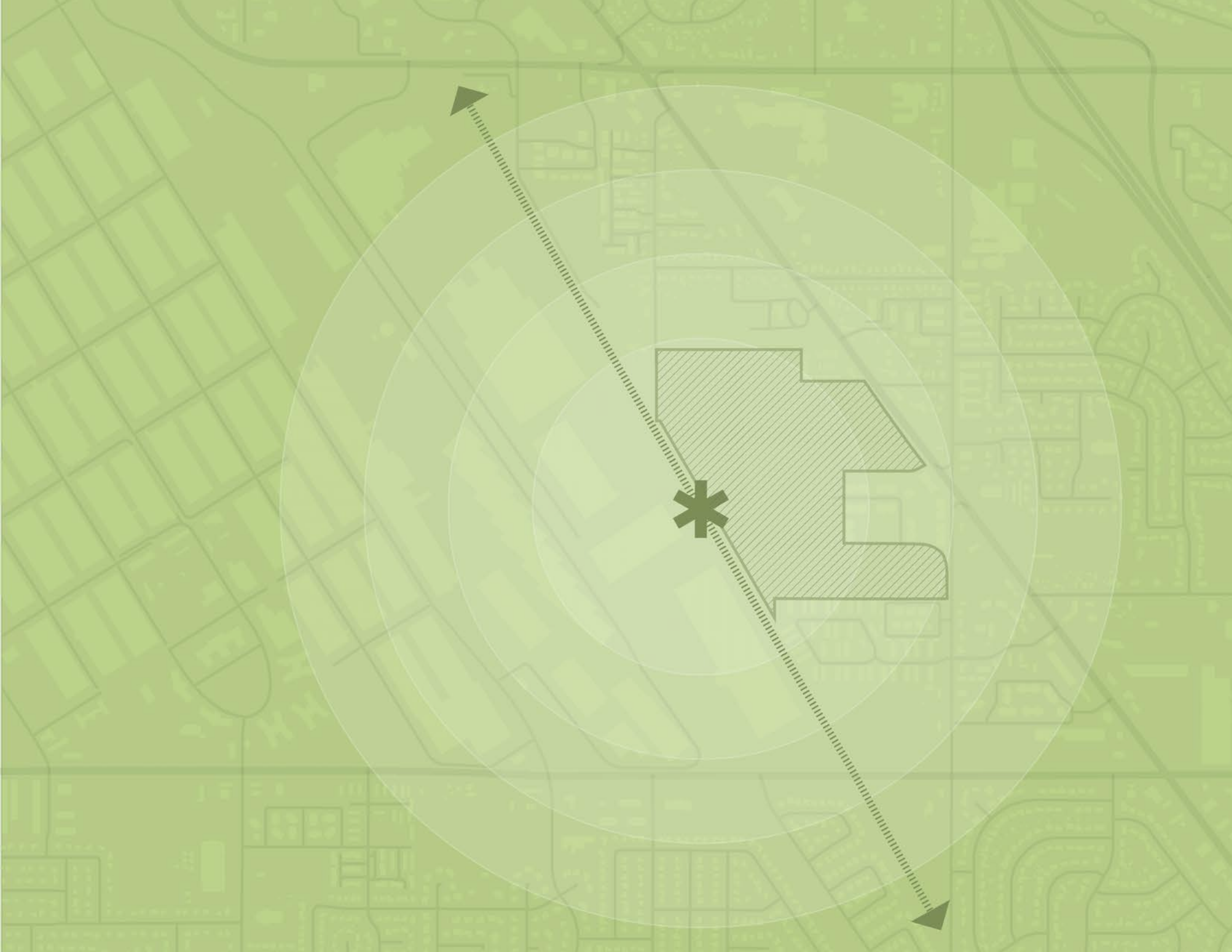
DESIGN GUIDELINES

- Pedestrian areas should have adequate illumination for safety.
- Lighting should be sensitive to residential development limiting glare, minimizing spill light, and minimizing light on upper stories of residential buildings.
- Retail buildings should integrate lighting with retail signage, storefront windows, and other building elements to enhance visibility and visual interest.
- Use creative lighting solutions to illuminate outdoor areas and add interest and life to outdoor spaces.
- All lighting should be dark-sky compliant.

PRECEDENTS

- 1 Ground floor transparency allows internal lighting to illuminate the street and creates a “glow.”
- 2 Lights on building exterior highlight the ground floor retail space and illuminate the street.
- 3 Light illuminates steps to promote pedestrian safety.
- 4 Lights used on canopy and sign add visual interest, as well as highlight the building entrance.
- 5 Overhead lights used to help create an interesting and exciting “place.”







06

OPEN SPACE + PUBLIC REALM

DESIGN GUIDELINES

Open Space Network

OVERVIEW

As part of establishing the Clearfield Station Area as a livable urban neighborhood, a high-quality, comprehensive open space network is essential. The Station Area will provide a variety of open space types to meet the needs of the various residents and visitors of the neighborhood. Open spaces will be provided in a variety of sizes and scales and will serve a range of specific functions. Most of this network will be part of the public realm and the remaining will be private, although all will contribute to the establishment of a unique and specific experience that complements one another.

INTENT

To create a comprehensive open space network that provides a unique yet unified system of parks and open

spaces throughout the neighborhood.

OPEN SPACE TYPES

The open spaces shown in the Illustrative Master Plan on the following page are conceptual. The specific intent for each is defined and illustrated in the pages that follow.

The district currently contains a large drainage basin in the southwest corner which will remain. The following open space types are outlined in this document:

- Park
- Pocket Park/Plaza
- Village Square
- Transit Plaza
- Woonerf
- Greenway
- Private Plaza + Open Space
- Cemetery

- Stormwater Basin
- Yards + Landscape Buffers

DESIGN GUIDELINES

- The open space network should provide a variety of open space types that complement one another.
- The open spaces should be integrated into the urban form of the neighborhood.
- Buildings should frame open spaces in a deliberate manner, rather than open spaces just being developed in the “leftover” spaces.
- The design and programming of each open space should reflect the latest trends in open space design to provide an experience and aesthetic that fits the wants and needs of the current day.
- Streets should be considered part of the open space network and should be designed in a

pedestrian-friendly manner that promotes comfort, safety, and provides places to stop and linger.

- Green infrastructure systems and ideas should be incorporated into the open space system.
- Buildings and respective land uses should work together with adjacent open space to provide uses that complement each other.

Park

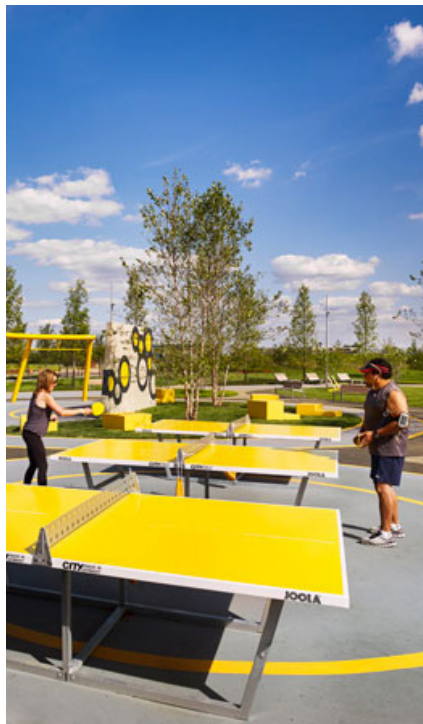
INTENT

To provide a public park space that is geared specifically toward residents in the neighborhood and functions like the backyard of the neighborhood where residents can relax and play in an informal environment.

FEATURES + ELEMENTS

The **Park** open space type should include:

- Children's playground and other play elements
- All ages play elements such as ping pong, pickleball, bocce, etc.
- Flexible lawn areas for informal active and passive recreation
- Pathway loops for exercise



Pocket Park / Plaza

INTENT

To provide a series of smaller parks and plazas that are typically located on small, irregular parcels, and are dispersed throughout the neighborhood. These spaces can serve as extensions of both the streetscape and the building.

FEATURES + ELEMENTS

The *Pocket Park/Plaza* open space type should include:

- Seating
- Interesting landscape design elements such as paving, planting, or other features
- Landscape features that reinforce the industrial theme for the neighborhood
- Outdoor dining seating (if applicable)
- Green space/planting to soften the urban environment



Village Square

INTENT

To provide a central open space of approximately 1 acre that is located in a highly visible area in the heart of the neighborhood. It should also become the primary gathering place for civic and social purposes, and should function as the living room for the neighborhood. This should become an iconic regional destination.

DESIGN GUIDELINES

The *Village Square* open space type should include:

- A strong image and identity that helps define the image of Clearfield Station.
- Framed by buildings with active ground floor uses that promote activity on the square.
- Iconic landscape features
- Flexible open gathering space for events
- Public art



Transit Plaza

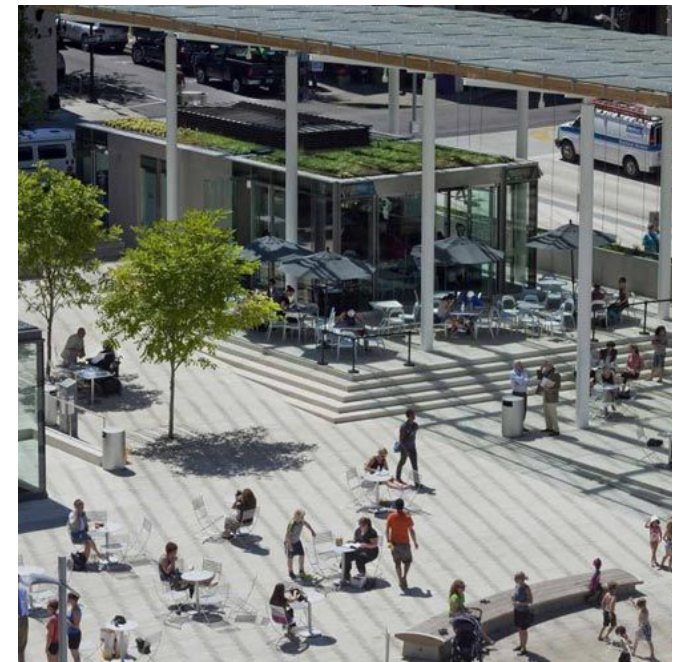
INTENT

To provide an open space adjacent to the commuter rail platform and bus loading zone that is specifically designed to enhance the experience of using public transportation by providing amenities that are geared toward transit users.

DESIGN GUIDELINES

The *Transit Plaza* open space type should include:

- Cafe, restaurant, or other convenient food options
- Public Restrooms
- Public art
- Seating
- Shade
- Landscape features that reinforce the industrial theme for the neighborhood.



Woonerf

INTENT

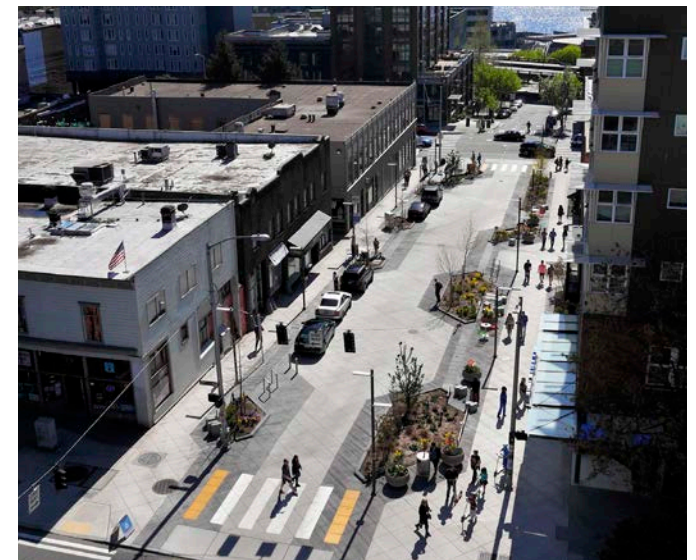
To provide a vibrant pedestrian-focused street, while still providing necessary vehicular access for businesses and residents.

FEATURES + ELEMENTS

The **Woonerf** open space type should include:

- Traffic calming
- Pedestrian-oriented design
- Enhanced streetscape
- Patio dining
- Street furnishings/amenities
- Removable bollards to close street to traffic during community events

Woonerfs provide important pedestrian connections, and are therefore also considered part of the transportation network. See Woonerf street type guidelines on page 116 for more detail.



Greenway

INTENT

To increase pedestrian connectivity between neighborhoods and to public open space, while also providing open space amenities for both visitors and adjacent residents.

FEATURES + ELEMENTS

The **Greenway** open space type should include:

- Pathways and trails
- Green space and trees
- Seating
- Small recreation activities
- Dedicated space for dogs and/or other pets



Private Plazas + Open Space

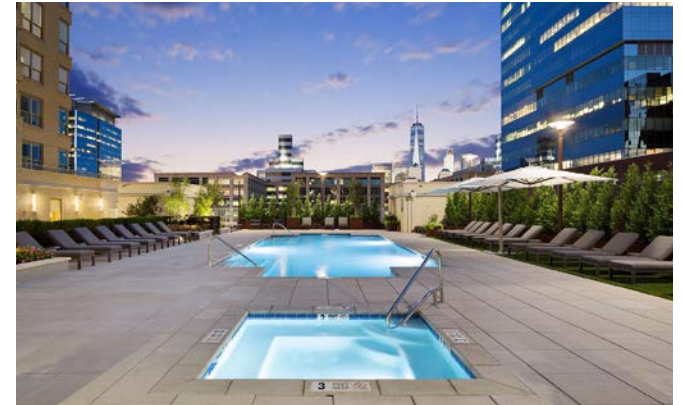
INTENT

To provide private open spaces for residents and/or employees of a building.

FEATURES + ELEMENTS

The **Private Courtyard / Rooftop Deck** open space type should include:

- Lounge and relaxation spaces
- Pools and hot tubs
- Outdoor cooking facilities
- Fire places
- Green space and trees
- Seating
- Small recreational activities
- Small private event gathering spaces



Yards + Landscape Buffers

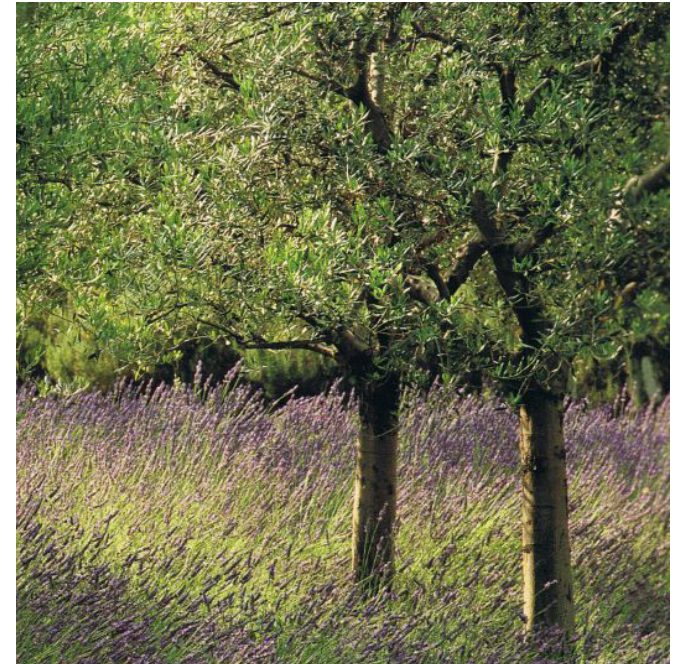
INTENT

To provide private yards and landscape buffers between buildings that are visually restorative while also being waterwise and environmentally appropriate.

FEATURES + ELEMENTS

The **Yards + Landscape Buffers** open space type should include:

- Waterwise Landscaping



Enhanced Streetscape

INTENT

To provide streets that are first and foremost designed to create a friendly pedestrian experience, in part by providing the appropriate pedestrian amenities.

FEATURES + ELEMENTS

The **Enhanced Streetscape** open space type should include:

- Seating
- Outdoor dining seating (where applicable)
- Landscape plantings
- Unique/Interesting paving
- Pedestrian lighting
- Public art integrated into functional streetscapes
- Street furniture such as trash/recycling receptacles, bollards, and more

See streetscape guidelines on pages 108-117 for more detail.



Landscape Design Theme

INTENT

To establish a specific “look and feel” to unify the area by developing a landscape “language” that will help brand the neighborhood with a unique aesthetic that also works with the architectural design.

DESIGN THEME - “CONTEMPORARY INDUSTRIAL”

The landscape design theme for the Station Area will mirror the architectural design theme with a contemporary industrial style that is modern, yet rooted in the industrial character that surrounds the area. This industrial character helps to create a brand for the area and provides a common theme that ties the neighborhood together.

LAWN AREAS

Lawn areas should be used strategically in areas that will become functional gathering places. Lawn areas should be minimized in other areas, and replaced with more water efficient landscape planting.



Materials + Colors

INTENT

To ensure a consistent application of complementary and high quality materials throughout the neighborhood that will reinforce the unique identity and a sense of place.

DESIGN GUIDELINES

- Landscape materials should reinforce the industrial theme by using concrete, steel, timber, brick and stone. See materials images for specific application of these materials.
- Utilize historic industrial remnants from the adjacent railroad, industrial area, and/or the historic navy depot, by integrating them into the landscape, if available.
- Materials are encouraged to have a weathered, industrial feeling. This could be done in various ways, such as using rough cut stone or concrete, or by using tumbled stone or brick. The weathered look should help create a feeling of “authenticity.”

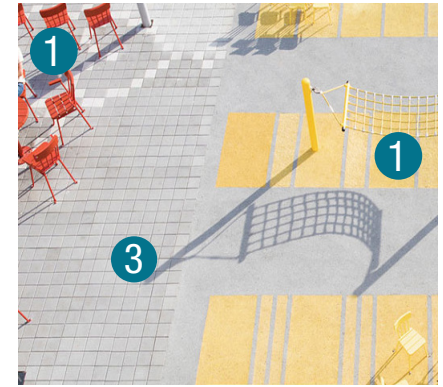
COLOR

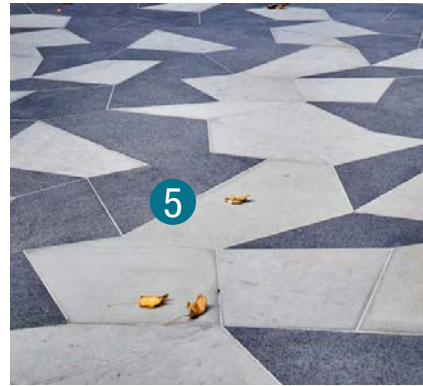
The most prominent color associated with industrial areas is gray, with reds and blacks also playing a large role. These colors should remain as a base for landscape material colors, but should also be supplemented with more modern and interesting colors. Specifically, brighter colors should be strategically added in minimal, but visually prominent ways, to contrast the muted gray tones.

- 1 Pop of Color as an Accent

ACCEPTABLE MATERIALS

- 2 Stone Pavers
- 3 Concrete Pavers
- 4 Broken Industrial Concrete
- 5 Abstract Industrial Broken Concrete
- 6 Decomposed Granite / Crusher Fines
- 7 Rough Cut Stone
- 8 Wood / Timber
- 9 Industrial Remnants (New + Old)
- 10 Steel / Railroad Track
- 11 Asphalt Pavers
- 12 Concrete / Board Form Concrete





Planting

INTENT

To reinforce the unique look and feel of the Station Area by utilizing planting in a way that is complementary to the contemporary industrial theme.

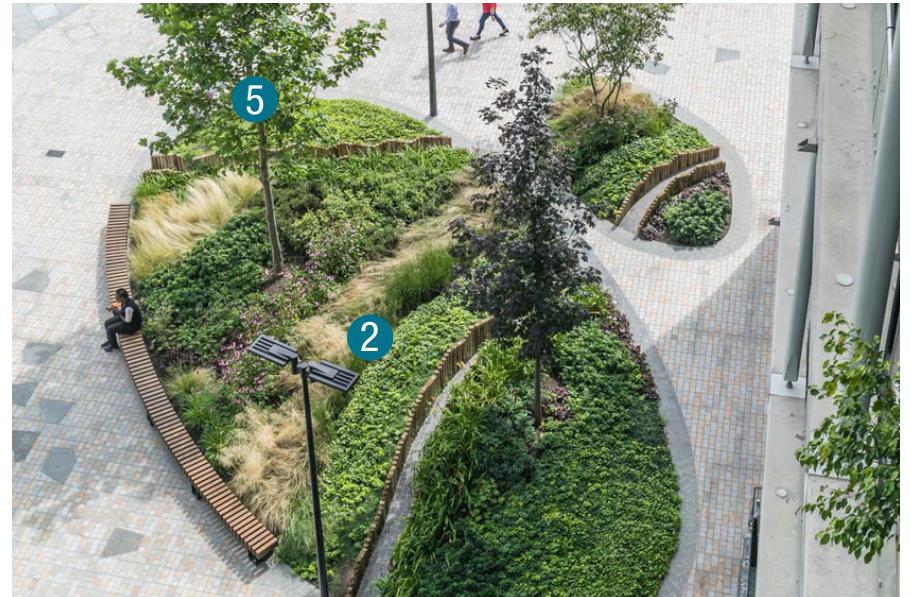
DESIGN GUIDELINES

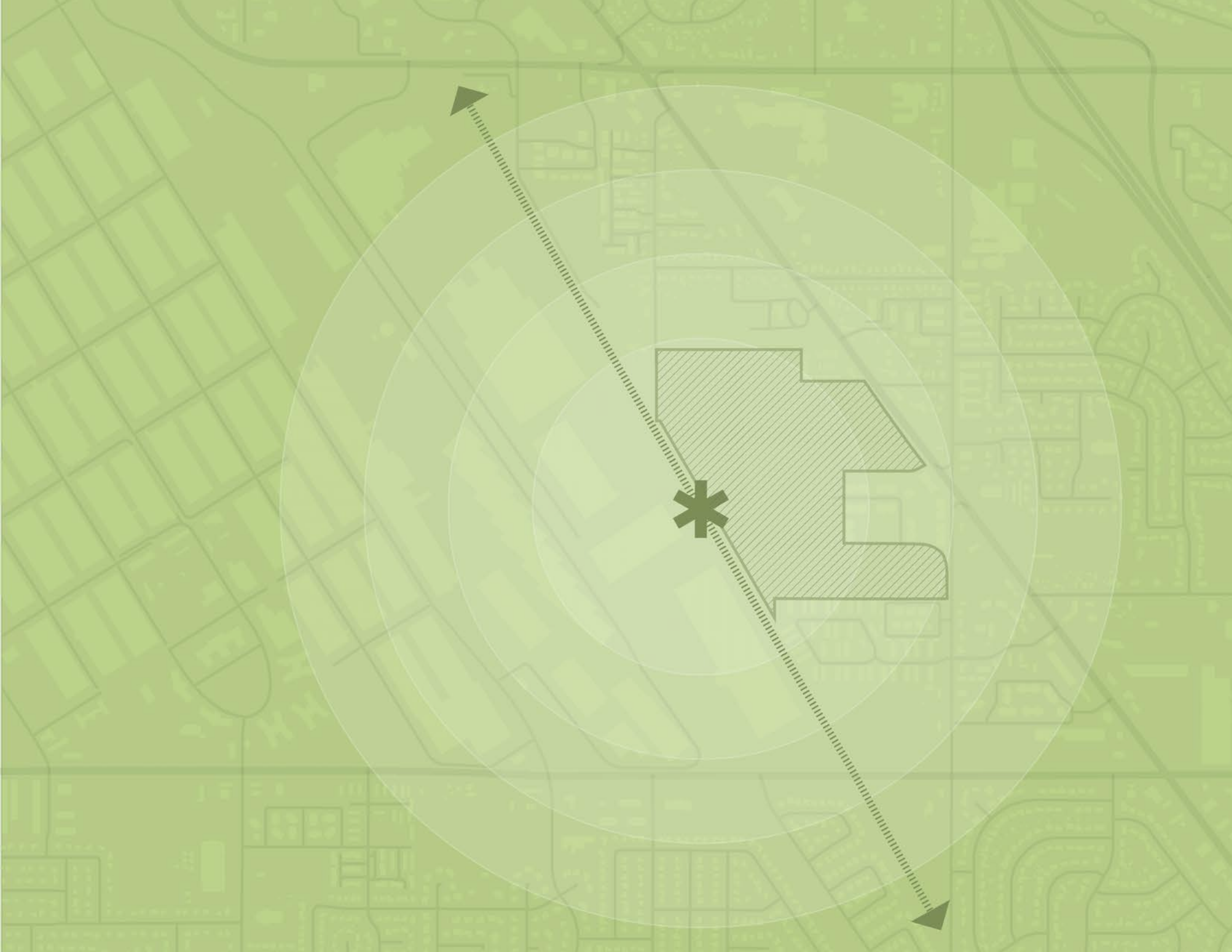
- Planting areas should generally have an organic feel.
- Planting in groups to create attractive massings is encouraged.
- Lawn areas should be used strategically in areas that will become functional gathering places. Lawn areas should be minimized in other areas, and replaced with more water efficient landscape planting.
- Use perennials, bulbs, and wildflowers to add color to the landscape.
- Choose plants that minimize long-term maintenance costs.

PRECEDENT

- 1 Organic Planting
- 2 Groups of Plants create organized massing
- 3 Naturalized meadows, native grasses, and perennials add color to the landscape.
- 4 Lawn area appropriately sized for gathering space.
- 5 Trees provide shade









07

TRANSPORTATION + MOBILITY

DESIGN GUIDELINES

Streets + Blocks

The street layout of the Clearfield Station District will provide the foundation for the urban form of the area, which will help define the character and performance of the neighborhood. Once established, the street pattern will remain in place as the long-term structure and framework for the area, even as buildings and land-uses may change and evolve over time.

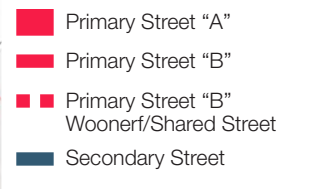
This layout incorporates the following:

- New streets connect into the existing street pattern to increase connectivity into the MDP site.
- Blocks are between 300' and 350' which is consistent with block sizes in successful, walkable downtowns throughout the country.
- The block size provides a good balance of ensuring good connectivity throughout the area, as well as providing a large enough block to allow for a variety of development options.
- Mid-block connections are encouraged to be designed into each block, if feasible, to further increase connectivity.



A hierarchy of streets has been established in order to define the most prominent and important streets in the neighborhood. It defines the various roles each street will play in regards to traffic volumes, modal choices, and pedestrian experience.

The street hierarchy specifically relates to the ground floor treatment of buildings, which is covered in Section 05 Buildings + Architecture of this document.

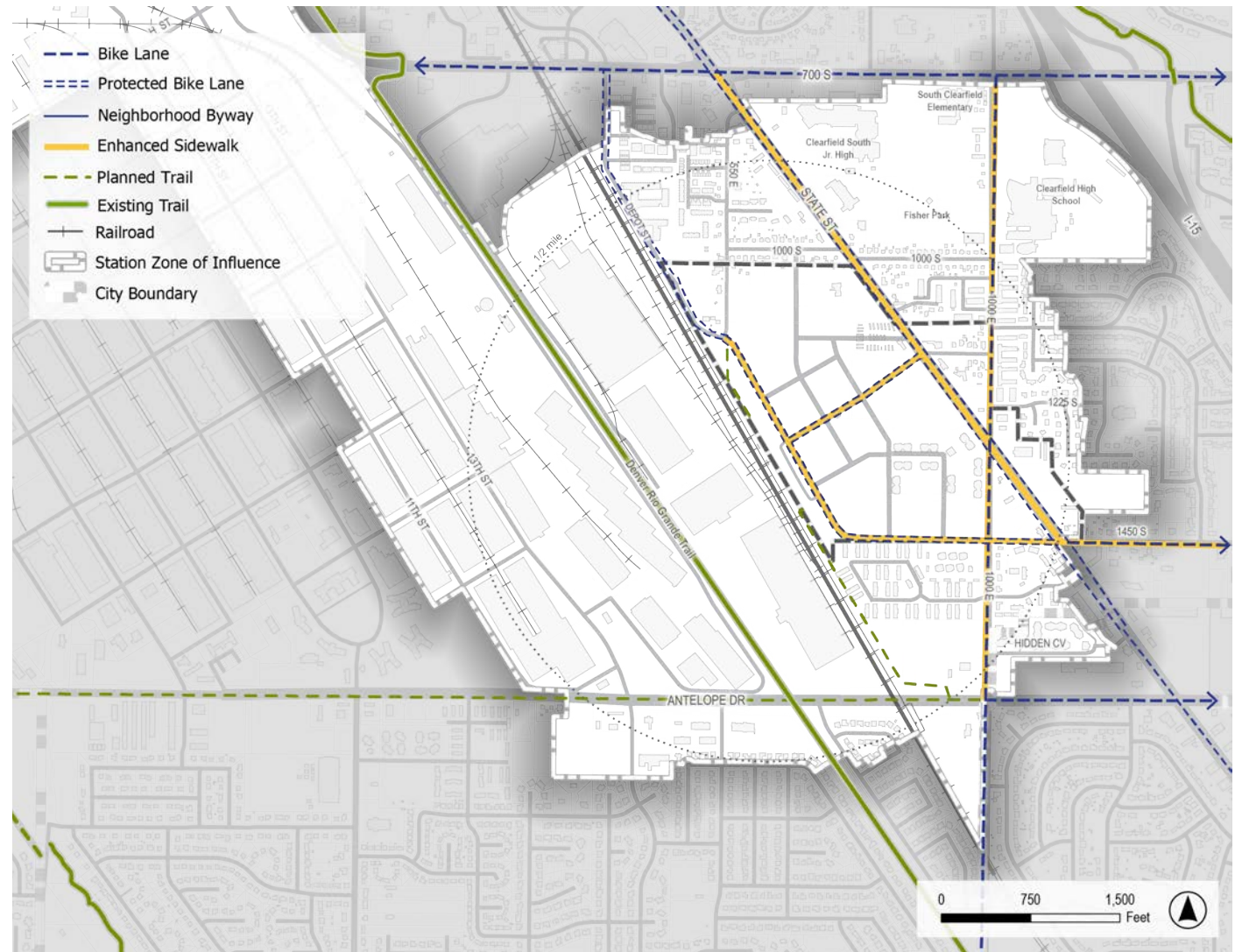


Active Transportation

Active transportation is defined as modes of travel that require physical effort. In Clearfield Station Area, this is specifically manifested as pedestrian and bicycle transportation.

Active transportation is an essential component of a transit-oriented development, as strong pedestrian and bicycle facilities allow transit users to connect from the train/bus to their destination with relative comfort and safety.

Quality active transportation facilities are also important for encouraging healthy lifestyles and reducing vehicle travel and congestion. The map on the right illustrates the proposed active transportation facilities for the station area.



PEDESTRIAN FACILITIES

The station area will specifically focus on providing pedestrian-friendly streets throughout the neighborhood. See street type guidelines on 107-117.

Special attention should be paid to ensuring highly visible and safe street crossings. Crosswalks should be located at all intersections within the area to enhance pedestrian connectivity.

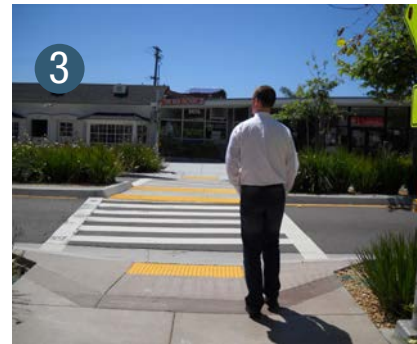
Bulb-outs (or curb extensions) should also be used throughout the neighborhood to calm vehicular traffic and shorten pedestrian crossings. Street trees should be used to increase pedestrian comfort and calm traffic.

CYCLING FACILITIES

Cycling facilities will be provided on primary streets within the Station Area. A protected cycle track will be provided on Station Boulevard. An on-street bike lane will run along Depot Street, through the MDP site, connecting to 1000 East. All other streets in the neighborhood will be designed to allow for a safe mix of cyclists and vehicles in vehicular travel lanes.

PRECEDENTS

- 1 Sidewalk with many elements that add to a comfortable, safe, and interesting pedestrian experience, including street trees, planters, brick pavers, ground floor transparency, pedestrian lighting, bike parking, seating, and outdoor dining.
- 2 Bulb-out helps to calm vehicular traffic and shortens pedestrian crossing lengths.
- 3 Highly visible crosswalk with median refuge and signage.
- 4 On-street bike lane with a painted buffer to increase safety.
- 5 Raised Cycle track separates bikes (and other users, such as scooters, skateboarders, etc.) from vehicular traffic lanes. It also separates these users from the pedestrian sidewalk space.



Transit

The commuter rail is the central feature of the Clearfield Station District, and planned development is arranged to maximize its use as a method for transporting people to and from the station, reducing the need for vehicular trips.

The commuter rail platform and entrances will remain. The bus loading zone will shift slightly to the South of Station Boulevard as seen in the map.

Bus traffic will largely be routed along the boulevard, with an option to exit on 1450 South. Bus routes with connection to Holy Cross Hospital - Davis will likely travel via 1450 South to and from the station platform.

A kiss-and-ride area will be established, as shown, to provide transit users from outside the neighborhood with convenient access to the commuter rail platform.

Transit facilities shall conform to UTA's design standards.



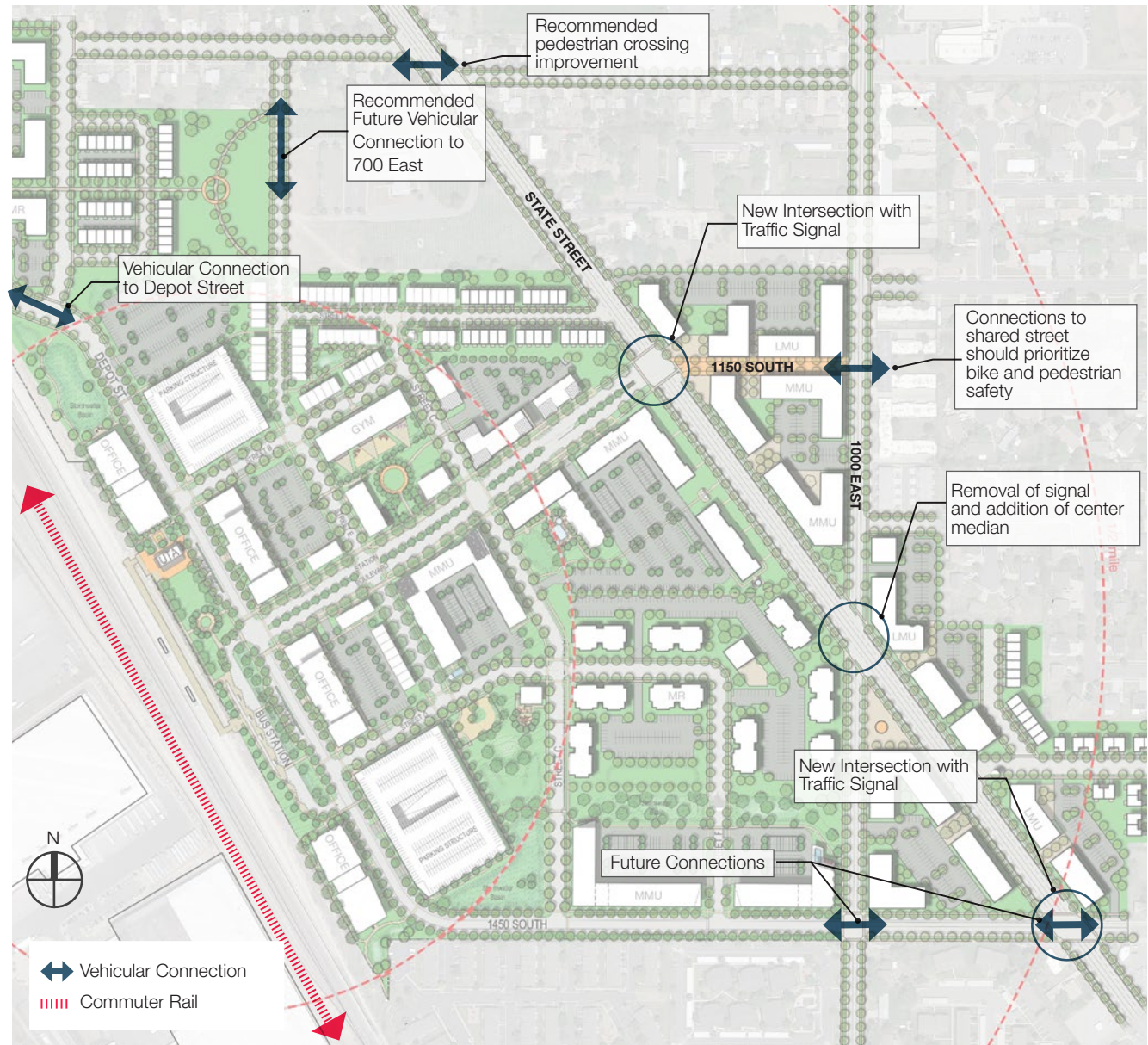
Vehicular Transportation

The Clearfield Station District is intended to be a multi-modal destination, with priority given to pedestrians and cyclists. However, vehicular transportation will still be a fundamental element that must be carefully planned to minimize traffic issues. The increase in development, as outlined in this plan, will have significant impacts on traffic, and traffic mitigation efforts must be carefully considered.

New streets should connect into existing streets to increase connections and to disperse traffic flows in and out of the area as much as possible. A connection to Depot Street should be prioritized. A connection to 700 East is also encouraged.

Improved pedestrian crossings on State Street at 1000 South, 1150 South, 1000 East, and 1450 South are recommended, in addition to a crossing at 1000 East and 1150 South. These connections are intended to overcome active transportation barriers and should prioritize bike and pedestrian safety.

The addition of traffic signals on State Street at Station Boulevard and 1450 South and the removal of the signal at 1000 East and State Street will likely have traffic impacts. The full impact of signal changes and/or removals will require further study and coordination with UDOT.



Traffic Analysis

A traffic impact analysis for the *Clearfield Station Area Plan* identifies the traffic impacts that the proposed land use scenario for the station will have in the surrounding intersections.

This traffic analysis is a 2023 update to the traffic analysis completed in 2019 by Fehr & Peers for the *Clearfield Station Area Plan*. It includes updated land use and trip generation assumptions for the Clearfield Station Area, including all parcels within ½ mile of the station that were not included in the 2019 analysis. All traffic volume growth assumptions and vehicle trip reduction percentages remain consistent with the 2019 analysis.

Trip generation for the project was computed using rates published in the Institute of Transportation Engineers (ITE) Trip Generation, 11th Edition, 2023.

The net external vehicle trips expected to be generated by the Clearfield Station Area, the percent reductions due to trips that start and end within the development, and trips that are done by transit, biking, or walking are shown in Table 5.

The Clearfield Station Area will generate significant traffic at the surrounding intersections, and mitigations will be needed to accommodate the new traffic. This analysis focused on the analysis of four intersections close to the Clearfield Station Area:

- State Street/2000 North
- State Street/1000 East
- State Street/Station Boulevard
- State Street/700 South

The operating performance of these intersections is described by the Level of Service (LOS). LOS is measured quantitatively and reported on a scale from A to F, with A representing the best performance and F the worst. See *Appendix B: Traffic Analysis* for descriptions of each LOS designation.

Using the traffic modeling software Synchro and the HCM 6 delay thresholds introduced above, the existing and existing plus project AM and PM peak hour LOS were computed for each study intersection. The preliminary results of this analysis are reported in Table 6.

TABLE 5: MXD TRIP GENERATION AND REDUCTION ESTIMATES

Time Period	Project Gross Trips	Net External Vehicle Trips	Vehicle Trip Reduction
Daily	30,319	26,226	13.5%
AM Peak Hour	2,002	1,616	19.3%
PM Peak Hour	2,888	2,221	23.1%

TABLE 6: LEVEL OF SERVICE SUMMARY

Intersection			Existing	Existing Plus Project	Existing Plus Project Mitigated
ID	Location	Period	LOS & Sec/Veh ¹	LOS & Sec/Veh ¹	LOS & Sec/Veh ¹
1	State Street / 2000 North	AM	D / 37	D / 37	D / 37
		PM	D / 41	D / 50	D / 50
2	State Street / 1000 East	AM	C / 26	C / 32	C / 26
		PM	D / 52	F / 96	E / 74
3	State Street / Station Boulevard	AM	B / 12	E / 47	E / 47
		PM	C / 19	F / >300	F / >300
4	State Street / 700 South	AM	C / 25	C / 27	D / 27
		PM	E / 63	F / 87	E / 58

1. Overall intersection LOS and average delay (seconds/vehicle) for the signalized intersections and worst movement LOS and average delay for the unsignalized intersections.

ANALYSIS RESULTS

All intersections in the existing conditions operate at acceptable levels during the AM peak hour (LOS D or better); however, **the State Street/700 South intersection operates at LOS E during the PM peak hour.**

With the addition of the proposed land use scenario for the Clearfield Station Area, the development access onto **State Street is LOS E** during the AM peak hour, and **all intersections except Main Street/2000 North operate at LOS E or F during the PM peak hour.**

The existing plus project scenario was also mitigated, i.e., the signals were optimized to provide better results. **This scenario shows significant improvements for the State Street/1000 East and State Street/700 South intersection during the PM peak hour.**

Therefore, it is recommended that the signals are optimized as the station area develops.

MITIGATION STRATEGIES

Other potential mitigations to alleviate the impact of the development on the surrounding area are:

- **Distribute internal traffic to all development accesses.** The main access to the development will be through State Street. However, three other accesses are proposed for this development: a south access onto 1000 East, and two north accesses, one onto 700 South (via Depot Street) and one onto 1000 South (via the recommended connection of 700 E). Encouraging the use of all development access points could alleviate the high traffic impact on State Street. However, a signalized access onto State Street might still be needed.
- **Signalize a secondary major access onto 1000 East.** 1000 East is a local road owned by Clearfield City. Adding a secondary major access onto this road will alleviate the traffic using access onto State Street.
- **Follow TOD best practices on parking supply.** Research conducted by the Utah Transit Authority and the University of Utah's Metropolitan Research Center indicates that mixed-use developments at transit stations generally require significantly less parking than similar developments that lack good transit access. The Utah Transit Authority also released Transit Oriented Development guidelines that provide standards for parking, although these guidelines provide a greater level of parking than the University of Utah research suggests to be necessary.
- **Establish a Transportation Demand Management (TDM) coordinator.** Having a TDM coordinator for the area would help employees and residents find other means of transportation to/ from the TOD beyond driving alone. Examples for TDM measures are incentivizing the use of transit, biking, and walking; having various office hours within the development; etc.

- **Optimize signals to improve PM peak hour LOS along State Street through the Clearfield Station Area.** The traffic analysis results showed significant improvement at signals along State Street when signal optimization was implemented. This strategy should be used at all signals in the station area to improve traffic conditions during peak hours.

Streetscape

INTENT

To create a cohesive, functional, and safe network of streets and walkways that supports a variety of travel modes and connects, attracts, and activates the neighborhood.

DEFINITION

The streetscape is defined in this document as the part of the street between the curb and the building.

DESIGN GUIDELINES

- The streetscape should be considered an important part of the neighborhood open space system, and should provide safe, comfortable travel, as well as interesting places that are desirable to spend time.
- Streets should be designed as outdoor rooms with attractive places to sit, stop, gather, and play.
- Streets should provide opportunities for neighbors and visitors to meet one another and create a vibrant community-oriented neighborhood experience.

- Paving materials and patterns should provide interest and excitement, while also being durable, functional, and easy to maintain.
- Changes in paving should be used to differentiate between streetscape zones.
- Curb radii should be minimized on street corners to slow vehicles making turning movements and maximize pedestrian safety.
- Bulb outs should be used at all intersections and mid-block street crossings to calm traffic and minimize the length of pedestrian crossings.
- Green infrastructure may be incorporated into the streetscape in the street zone with stormwater retention systems or other innovative green systems.

BUILDING ZONE

The building zone is the space between the travel zone and the building facade. This zone can be used to display merchandise, enhance entryways, or provide outdoor seating and dining. It should generally be thought of as an extension of the building into the public realm. This space will typically require some space from a building setback to provide enough usable space.

TRAVEL ZONE

The travel zone is reserved for unobstructed pedestrian travel. It is located between the building zone and the street zone. The National Association of City Transportation Officials (NACTO) recommends 5-7 foot wide sidewalks in residential areas, and 8-12 foot wide sidewalks in downtown areas.

STREET ZONE

The street zone is the space between the travel zone and the street. This area can be landscape or hardscape, and is where trees and street furniture should be located.

STREET TREES

Street trees are required in regular intervals on all streets in the neighborhood. They should be located at least 30 feet apart.

STREET FURNITURE

Street furniture should be provided as part of the general streetscape design for all streets in the neighborhood. The following list includes street furniture that should be included within the Clearfield Station Area. However, not all streets will require all street furniture elements.

- Street Lighting
- Pedestrian Lighting
- Seating / Benches
- Trash / Recycling Receptacles
- Bike Racks
- Wayfinding Signage
- Raised Planters
- Bollards



GENERAL STREETScape ELEMENTS

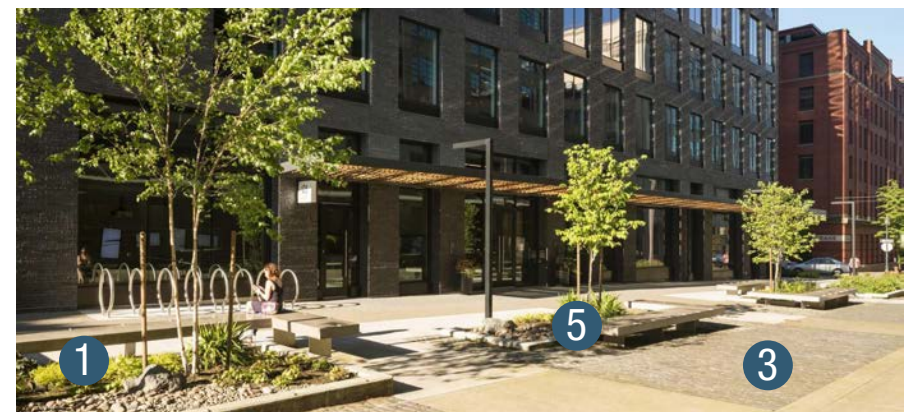
Streetscape design is key in creating an inviting pedestrian environment and a walkable neighborhood.

This graphic demonstrates how the three streetscape zones are broken down, and the simple fundamentals behind effective street design.

- 1 A consistent streetwall on both sides of street, as well as vertical elements such as trees, create a sense of enclosure.
- 2 A consistent row of trees provides a sense of enclosure, protects pedestrians from vehicles, provides shade, and brings nature into the urban environment.
- 3 Street furniture such as lighting, seating, trash receptacles, and bike racks are included in the street zone as pedestrian amenities.
- 4 Seating and outdoor dining is provided in the building zone as an extension of the indoor dining area.

STREETSCAPE PRECEDENTS

- ① Street zone contains trees, plantings and street furniture.
- ② Building zone contains pedestrian amenities such as outdoor dining.
- ③ Interesting paving pattern brings excitement and refinement to the street
- ④ Bioretention strip is built in to the street zone of the streetscape to filter stormwater.
- ⑤ Seating is designed into interesting streetscape planters.



Street Types

Five street types have been established for the Clearfield Station District.

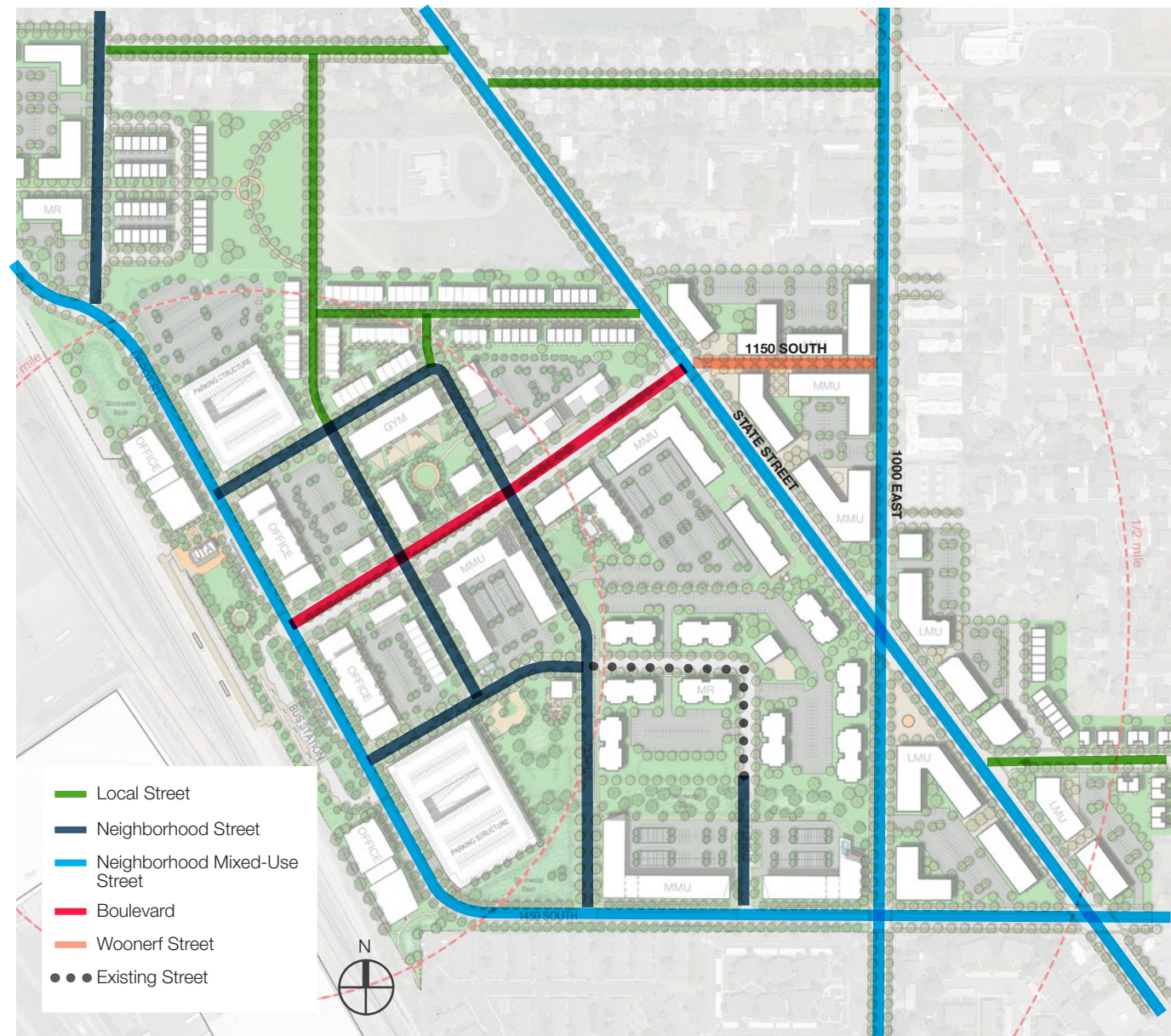
The **Local Street** is a low-speed and low-volume street for connecting neighborhoods to connector streets

The **Neighborhood Street** is the default street design, and the most common street in the neighborhood.

The **Neighborhood Street - Mixed-Use** street type is identical to the "Neighborhood Street," but has dedicated on-street bike lanes.

The **Woonerf** type is a shared street that prioritizes pedestrians and bicycles over motor vehicles and provides flexible space for outdoor dining, street festivals, and food trucks.

The **Boulevard** street type is established as the primary street in the neighborhood, which connects State Street to the transit station.



Local Street Type

The **Local Street** type is primarily used on residential-only streets within and beyond the Station District. Similar to Neighborhood Streets, Local Streets are intended to provide access for neighborhoods and function as a livable outdoor space but on lower volume, quieter streets.

The local street type includes street trees, plantings, and sidewalks.

This street section is designed for a slow speed, which allows bicycles to safely and comfortably share the vehicular lanes.





Neighborhood Street Type

The *Neighborhood Street* type is the default street type that will be used in the station area and will make up the majority of streets in the neighborhood. It is intended to provide access for neighborhoods and function as a livable outdoor space. The design and layout of the street is a simple, time-tested solution that creates safe, walkable, and livable streets.

The neighborhood street type includes on-street parallel parking, street trees, plantings, lighting, benches, and sidewalks.

This street section is designed for a slow speed, which allows bicycles to safely and comfortably share the vehicular lanes.





Neighborhood Mixed-Use Street Type

The *Neighborhood Mixed-Use* street type is identical to the *Neighborhood Street* type, with the exception of adding on-street dedicated bike lanes.

The buffered bike lanes on these streets will provide safe and convenient access for bicycles on the streets that connect the station area to the rest of the City.



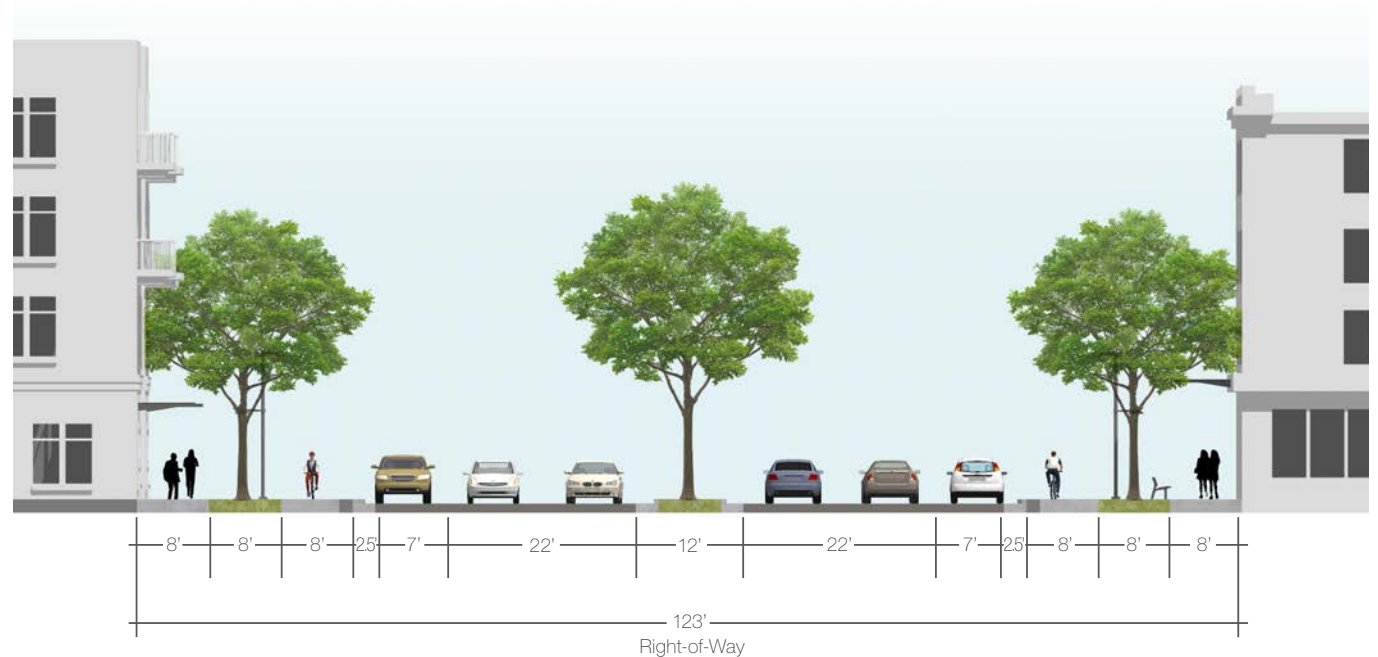


Boulevard Street Type

The *Boulevard* street type is intended to be the “Main Street” for the Clearfield Station District, connecting State Street to the Transit Station.

Station Boulevard should be designed to have a grand, iconic appearance, as it is the main entrance to the neighborhood and the heart of the station area. It should be designed to be functional, safe, and convenient for multiple modes of travel, including vehicles, bus, bicycles, and pedestrians.

The Boulevard street type includes on-street parallel parking, street trees, plantings, a planted median, sidewalks, lighting, benches, and other street furniture.



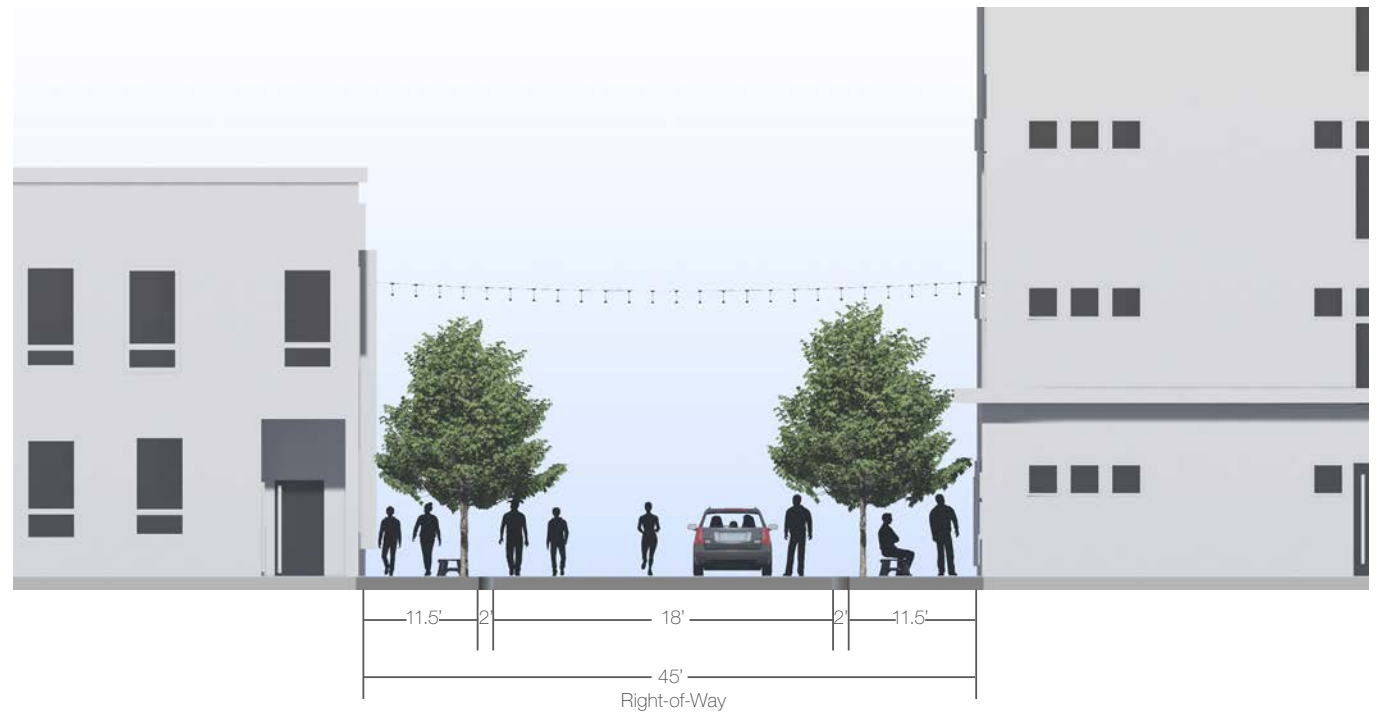


Woonerf Street Type

A woonerf or shared street is a Dutch concept that reimagines the street as a shared space for pedestrians, bicyclists, and motor vehicles, with pedestrians and cyclists having priority over cars.

The current right-of-way on 1150 East is 45'. The proposed woonerf will utilize elements such as trees, planters, benches and other furnishings, and drainage grates to break up sight lines, provide shade and protection.

Because the woonerf is not a priority route for motor vehicles, this street could be easily closed for special events such as street festivals, food truck rallies, block parties, or other neighborhood events.





Parking

OVERVIEW

A comprehensive strategy to deal with parking is one of the most important aspects of creating a successful, walkable, TOD environment. The majority of parking in the Station District will be provided on surface lots and structures with some on-street parking.

Park and ride, visitor, and ADA parking shall be prioritized and located within the shortest distance possible. Landscaping should be used to screen parking from the street where possible.

The parking plan provides about 3,400 parking stalls, which give parking flexibility for future businesses.

INTENT

To arrange parking in a way that promotes walkability, while still providing convenient and accessible parking.

DESIGN GUIDELINES

- Dedicated parking structures will provide parking for park and ride purposes.
- Adequate bike parking should be provided for each building in the neighborhood.

- Parking structures facing Depot Street and 1450 South may have active uses on the ground floor.
- All streets are to include on-street parking where possible.
- Shared parking strategies are encouraged.
- Office parking shall have a minimum of 5% of parking stalls to be Electric Vehicle (EV) hook up ready and at least four stalls per 150,000 SF built.
- EV charging stations to be 220/240 volt minimum (Level 2).

PARKING STRUCTURE PRECEDENTS

- 1 Parking Structure is wrapped by buildings to hide the parking structure from the street and public open spaces.
- 2 Retail uses on the ground level of parking structure activates the street.
- 3 Decorative facade treatment of parking structure adds visual interest to the street.



CONCEPT PLAN PARKING

The plan to the right illustrates a number of parking configurations and strategies that could be used to provide parking in the Station District. Some of these include, but are not limited to:

On-Site Residential Parking:

Surface-level parking located directly adjacent to planned residential uses.

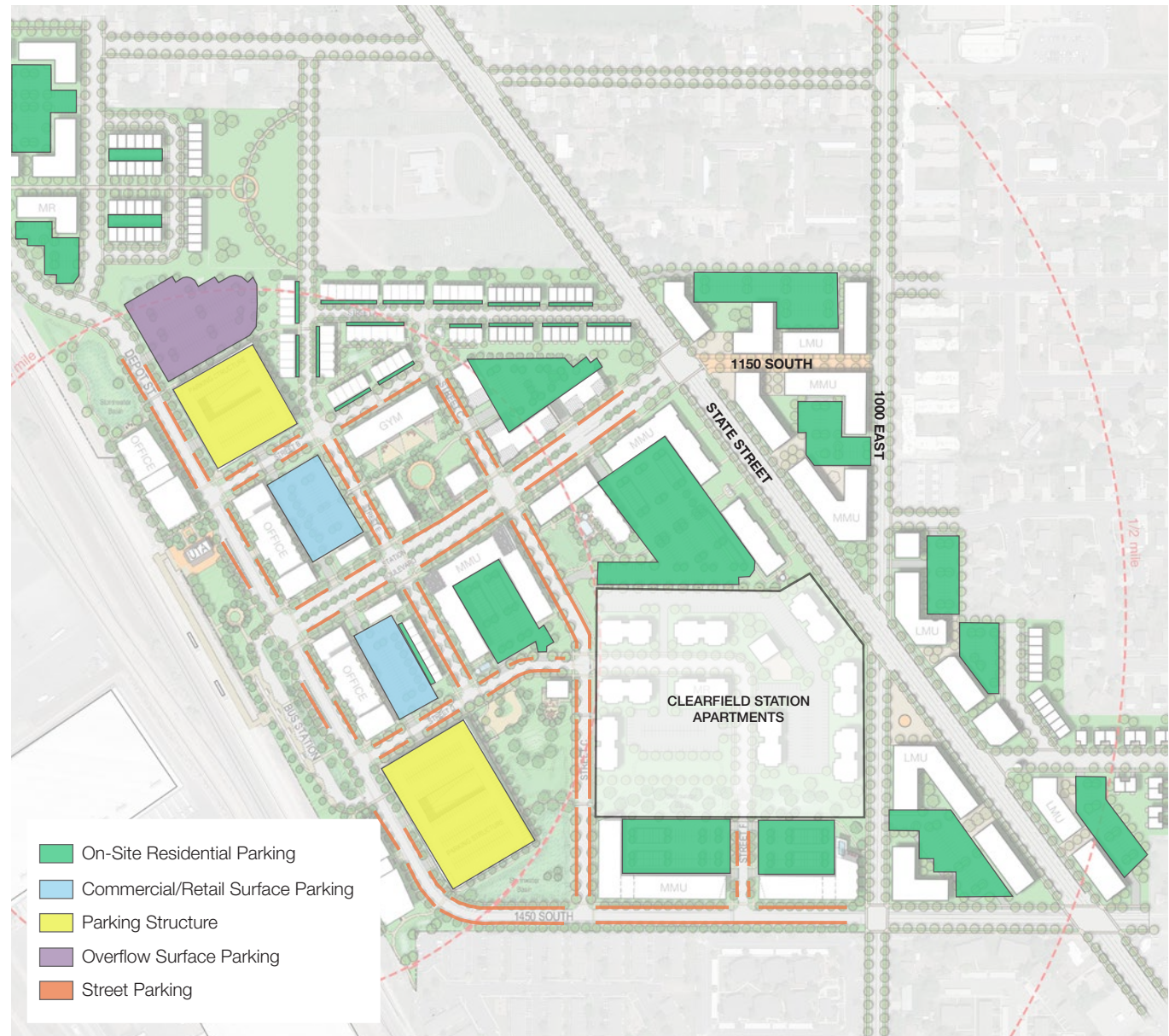
Commercial/Retail Surface

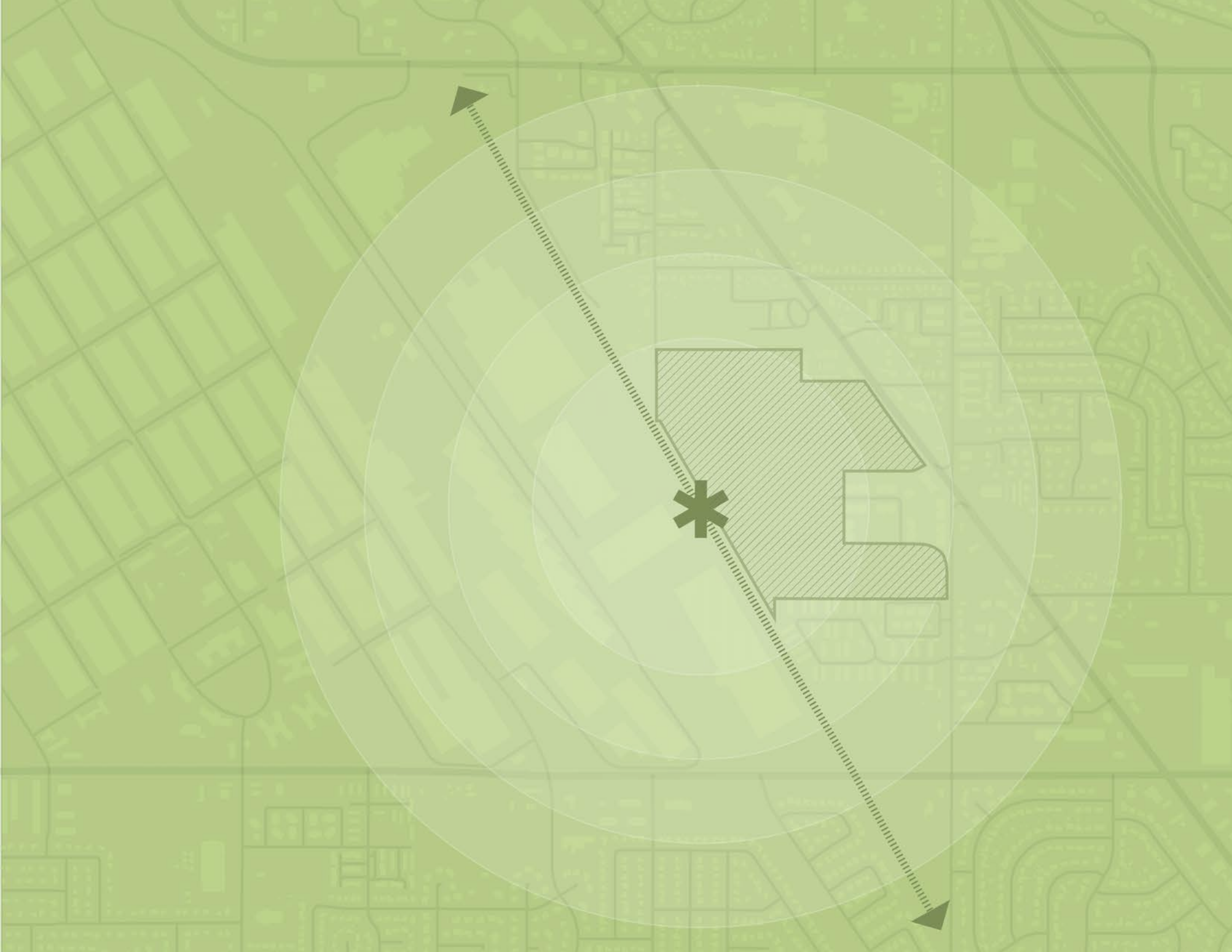
Parking: Surface-level parking located directly adjacent to planned commercial uses.

Parking Structure: Located either above ground with active uses on the ground floor or underground.

Overflow Surface Parking: Surface-level parking utilized when the existing parking supply does not meet demand.

Street Parking: All streets are to include on-street parking where possible.







08

STRATEGIC RECOMMENDATIONS

Implementing the Plan

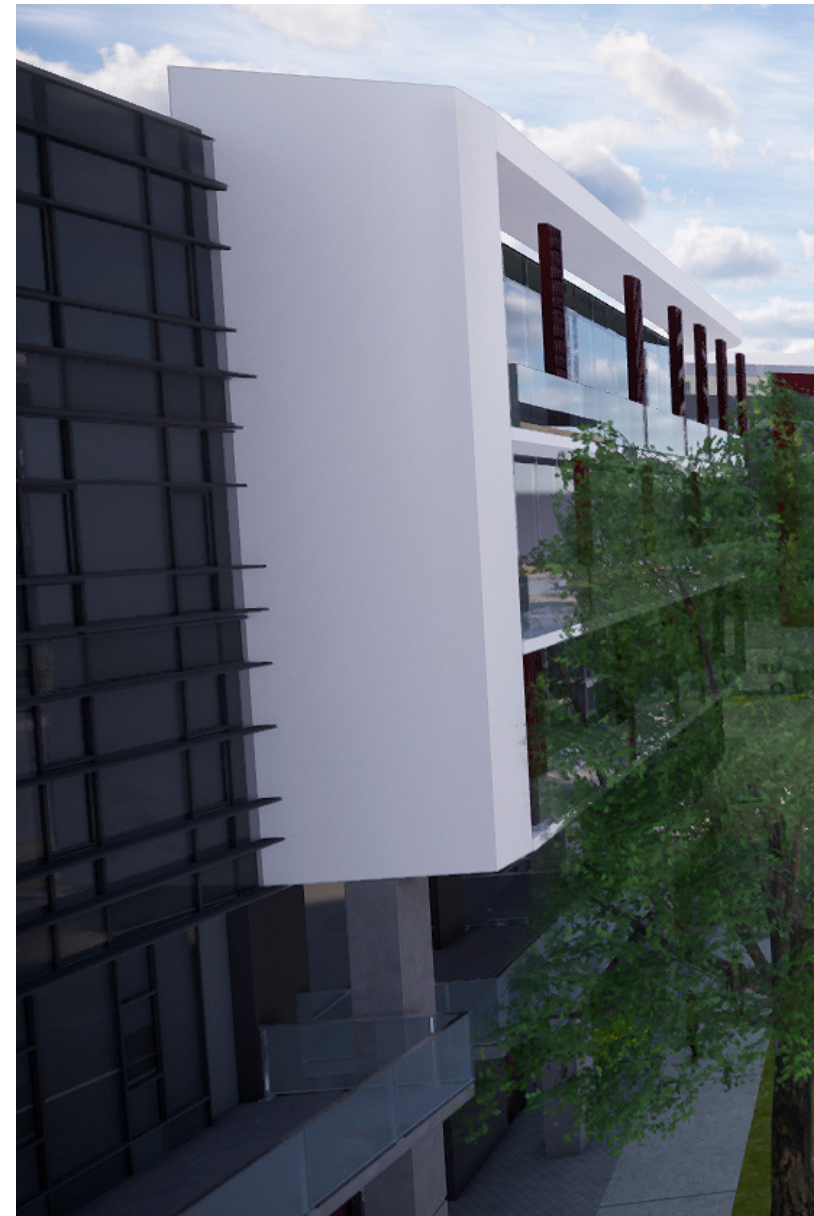
For the vision and objectives laid out in this plan to be realized, it will likely be the result of a long-term process, where residents, City Staff, UTA Staff, and elected officials have championed the vision and ensured the development of the area that they want to see. This plan presents the vision and illustrative plan for the Clearfield Station Area, but for the type of development this plan envisions to be built, more steps will need to be completed.

The strategic recommendations outline the next steps for the Station Area. They are intended to provide the action items that the City, UTA, or other stakeholders must complete to be ready for implementation. Not all steps must be completed before development on the area can begin, but each step will need to eventually be completed to ensure the area reaches its potential as outlined in this plan.

The strategic recommendations are broken down into four categories:

- Policy Updates + Plan Amendments
- Economic Development
- Transportation
- Physical Improvements

The image on the following page illustrates how the project area might look at buildout.





Policy Updates + Plan Amendments

- ☐ Ensure consistency between the Clearfield Station Area Plan and other planning and regulating documents
 - The Clearfield General Plan should be evaluated and revised as necessary to ensure consistency with this plan.
 - The City's streets and trails plans should be updated to be consistent with this plan.
- ☐ Update the City's Future Land Use Map and Zoning Ordinances to reflect the proposed land uses indicated within this plan (See map on page 37).
- ☐ Create a form-based code for the Clearfield Station District (see Districts Map on page 33), basing the requirements on the architectural design guidelines established in this plan.
- ☐ Update city transportation policies to include street and transportation related design guidelines as outlined in this plan.
- ☐ Investigate and implement strategies to incentivize or require affordable housing within the Station Area, coordinating closely with the City's Moderate Income Housing Plan.
- ☐ Develop a brand for the area
 - Establish a unique brand for the Station Area that will increase visibility and help the area become more attractive to developers, future residents, and employers/employees.
- ☐ Develop refined site plans for undeveloped properties outside of the existing MDP.
 - Develop site plans describing the physical location of buildings, accesses, and parking within the proposed developments. This plan should reflect the finalized design of the development for approval by Clearfield City, UTA, and/or UDOT once a developer is ready to develop the land.

Economic Development

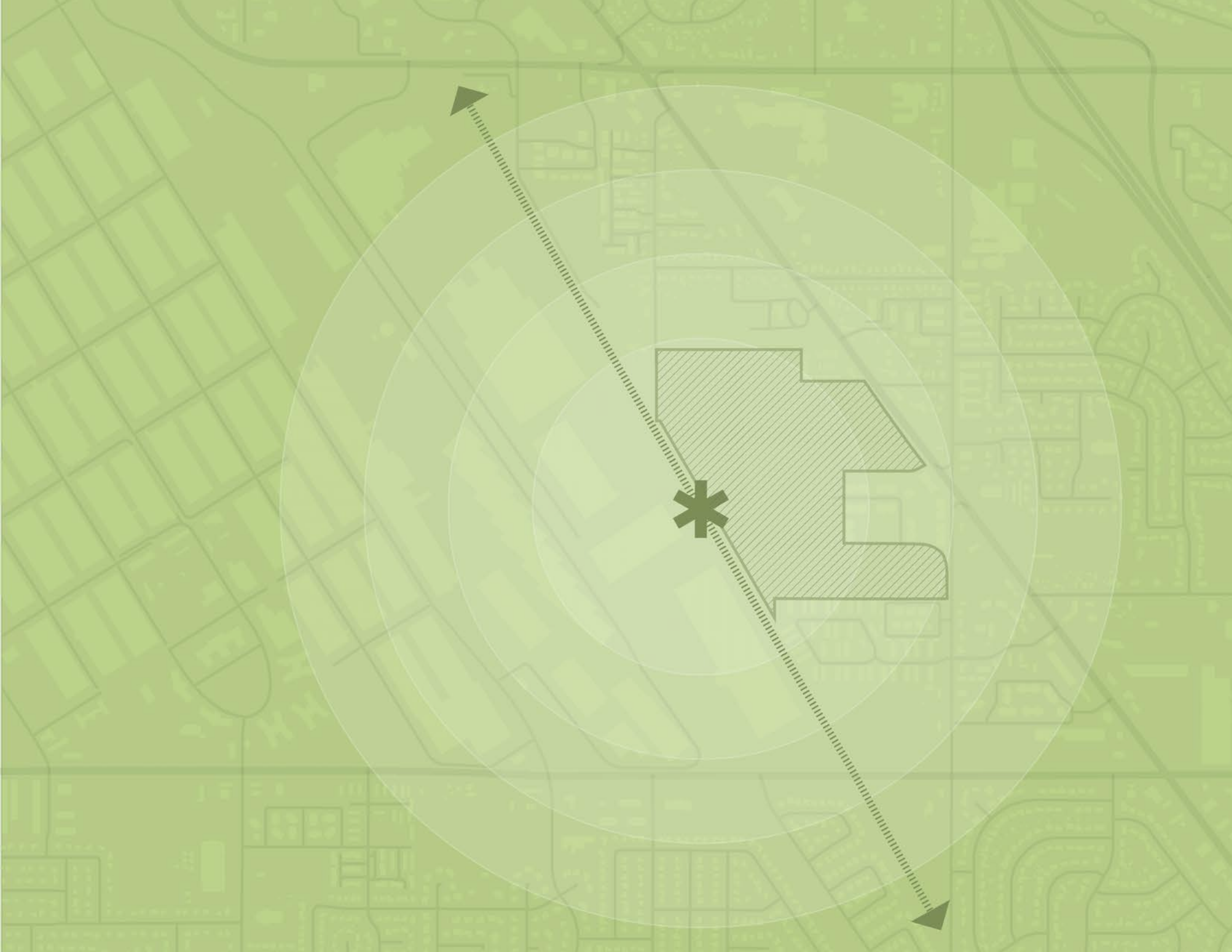
- ☐ Consider formation of a Transportation Reinvestment Zone (TRZ)
 - A TRZ is similar to the existing CDA, in that it is a program that utilizes tax increment financing. However, the advantage to the TRZ is that the majority of the funds can be used for transportation improvements. It also removes the requirement of setting aside ten percent of the increment for affordable housing.
- ☐ Reevaluate retail buying power
 - As new residential product is introduced into the area, the City should consistently reevaluate the retail buying power potential. That actual, or even planned growth, can be translated into specific buying power in terms of real dollars. That information needs to be used in attracting new retailers to the overall area.
- ☐ Reevaluate the fiscal impacts of use types
 - The City should regularly reevaluate the fiscal impacts of use types to reconsider their municipal cost models and make changes as market conditions affect different real estate Sectors.
- ☐ Solicit development partners and commercial tenants
 - UTA and the City should actively solicit development partners and commercial tenants who share the vision for the Clearfield Station Area.

Transportation

- ☐ Further Study impacts of the addition of Station Boulevard signal and changes to 1000 East signal
 - The proposed and potential signal changes will impact traffic patterns and delays, but further study is needed to know the full impacts of these intersection changes.
- ☐ Improve pedestrian crossing at 1000 South and State Street
 - If possible, relocate the existing HAWK signal from its current position north of 1000 South on State street to the crossing between 1000 South and Campbell Heights across State Street.
- ☐ Prioritize bike and pedestrian connections to shared street
 - The proposed shared street (1150 South) will be designed to prioritize bicycle and pedestrian traffic, therefore bicycle and pedestrian connections to this street should be prioritized.
- ☐ Complete an Operational Analysis and Circulation Plan
 - Due to the high-density development of the Clearfield Station Area, an internal operational analysis should be completed to determine the type of traffic control needed within the development (two-way stop control, four-way stop control, free, roundabouts, traffic circles, etc.).
- ☐ Develop parking strategy
 - Develop a strategy for parking that takes into account opportunities for shared parking, phasing, and other innovative strategies to provide parking for employees, residents, and visitors.
- ☐ Continue to work with UDOT to improve safety and connectivity across State Street.

Physical Improvements

- ☐ Work with UTA, project stakeholders, and potential developers to implement the Clearfield Station Master Development Plan (MDP).
- ☐ Implement the active transportation and trail infrastructure proposed in this plan.
 - Safely and efficiently connect the D&RGW Trail to the Clearfield Station.
 - Extend trails and make pedestrian connections to other areas of the city.
- ☐ Transform 1150 South into an extension of Station Boulevard.
 - Conduct a design & feasibility study for converting the street into a woonerf, including a realignment of the intersection with State Street and Station Boulevard. Design and construct the proposed woonerf based on the results.
- ☐ Design and construct the proposed public parks indicated in this plan (see page 79).
- ☐ Enhance streetscapes within the Station Area through consistent street trees, improved landscaping, street furnishings, and lighting.
- ☐ Allocate tax increment to construct parking structures near station platform to provide park & ride parking for transit users.

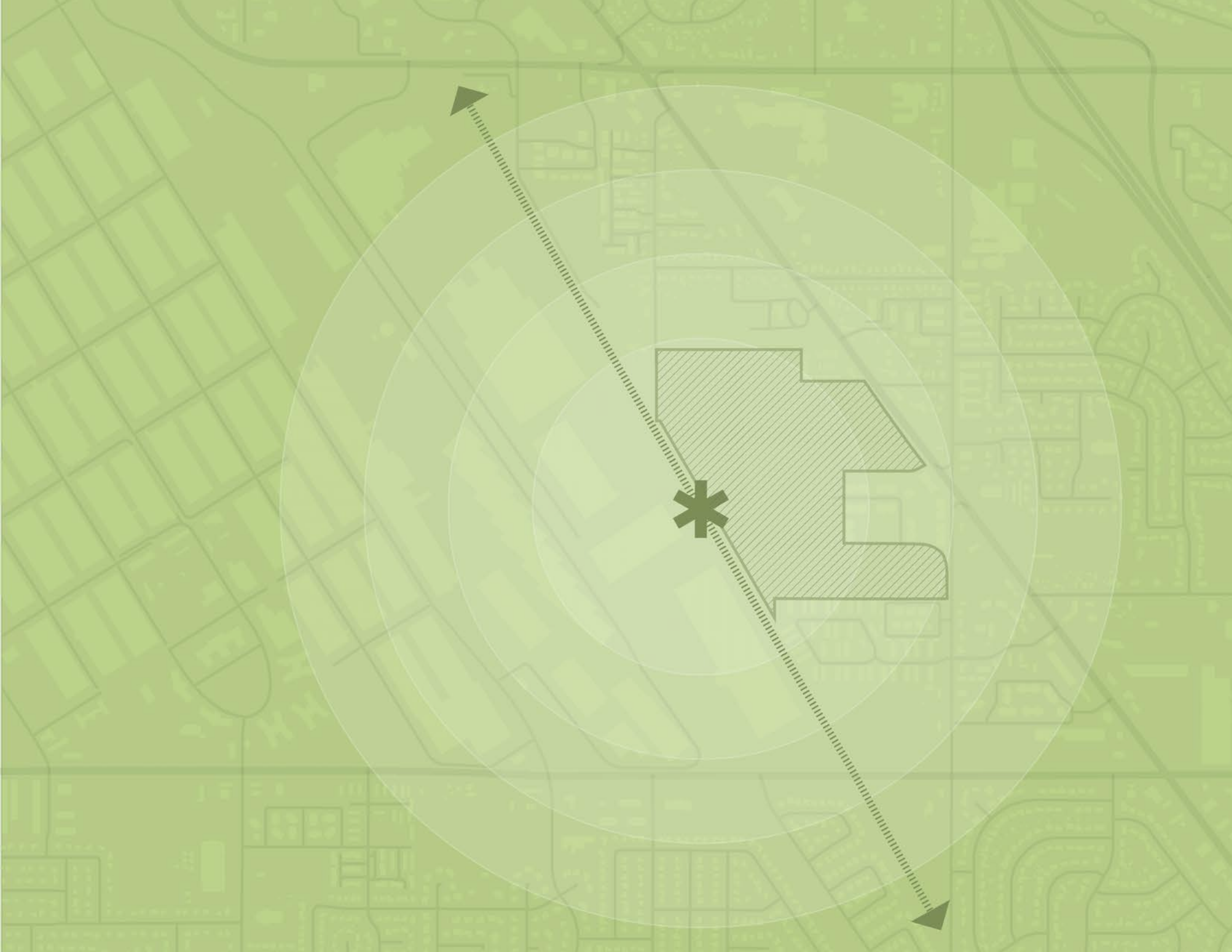




09

Appendix A

*Existing Conditions Report:
Land Use & Transportation*





10

Appendix B

*Existing Conditions Report:
Market & Housing*

